



TRANSPORTATION CABINET

Frankfort, Kentucky 40622
www.transportation.ky.gov/

Steven L. Beshear
Governor

Michael W. Hancock, P.E.
Secretary

May 10, 2011

CALL NO. 200
CONTRACT ID NO. 111024
ADDENDUM # 2

Subject: Christian County, 024GR11D024-IM & JL03
Letting May 20, 2011

(1) Revised - Plan Sheets - R2, R2A, R2B, R2C, R2D, R2F, & R2H

Proposal revisions are available at <http://transportation.ky.gov/contract/>.
Plan revisions are available at <http://www.lynnimaging.com/kytransportation/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

A handwritten signature in blue ink that reads "Ryan Griffith".

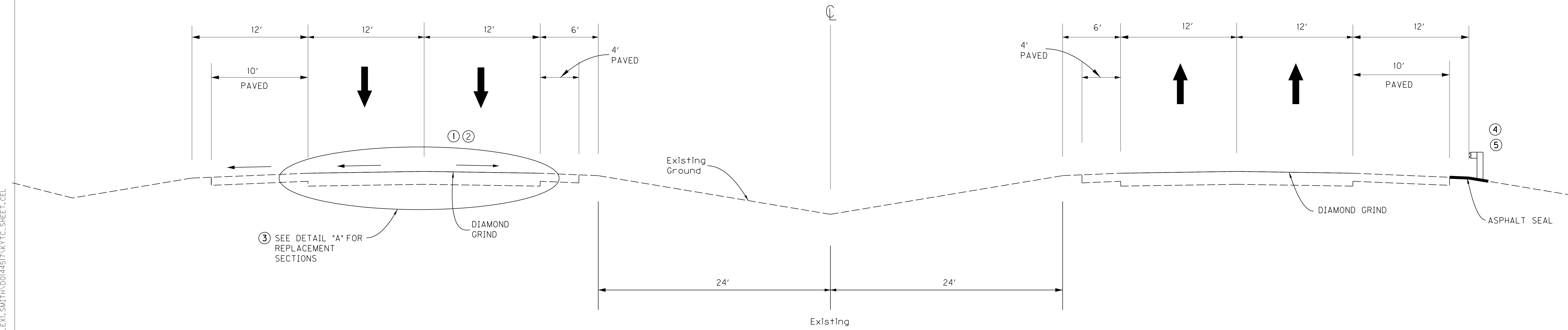
Ryan Griffith
Director
Division of Construction Procurement

RG:ks
Enclosures



An Equal Opportunity Employer M/F/D

I-24 / US 41A INTERCHANGE I-24 TYPICAL SECTIONS



NORMAL SECTION – I-24

STA. 5709+38 TO STA. 5731+32

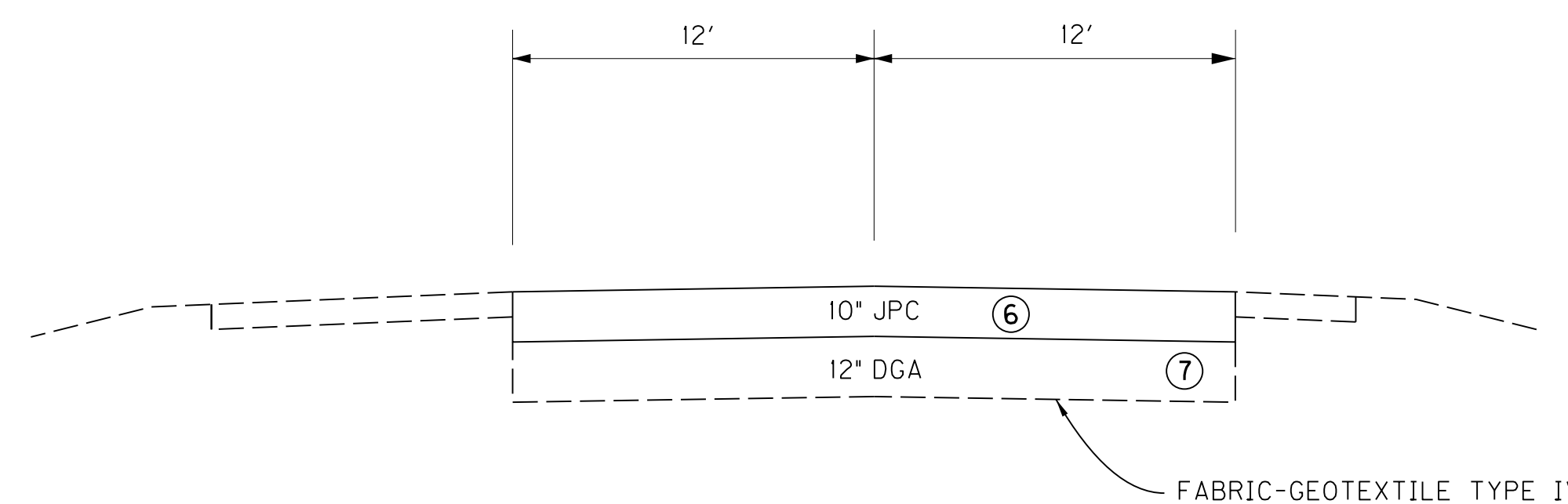
I-24 NOTES

- ① MAINLINE DRIVING LANES TO BE DIAMOND GRIND FOR BOTH EASTBOUND AND WESTBOUND I-24.
- ② ALL JOINTS TO BE RESEALED
- ③ PAVEMENT REPLACEMENT LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER AND ARE TO BE IN ACCORDANCE WITH THE PROVIDED DETAILS AND STANDARD SPECIFICATIONS.
- ④ REMOVE EXISTING GUARDRAIL AND REPLACE. NEW GUARDRAIL TO HAVE 7' POST. SEE GUARDRAIL SUMMARY SHEET FOR LOCATIONS.
- ⑤ IN GUARDRAIL REPLACEMENT AREAS:
ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE.
TWO APPLICATIONS OF THE FOLLOWING:
EMULSIFIED ASPHALT RS-2 2.4 LBS/SY
ASPHALT SEAL AGGREGATE 20 LBS/SY
- ⑥ THROUGH THE LIMITS OF THE REPLACEMENT SECTIONS, EXISTING CONCRETE PAVEMENT TO BE REMOVED AND REPLACED WITH 10" JPC.
- ⑦ IF UPON EXCAVATION OF THE EXISTING CONCRETE PAVEMENT THE RESIDENT ENGINEER DETERMINES THE SUBGRADE TO BE UNSUITABLE, THE CONTRACTOR WILL EXCAVATE 12", OR DEPTH DIRECTED BY ENGINEER, AND CONSTRUCT EQUIVALENT DEPTH OF DGA OVER FABRIC-GEOTEXTILE TYPE IV.

PAVEMENT REHABILITATION RIGID PAVEMENT – DRIVING LANES

- DIAMOND GRIND EXISTING SURFACE

REPLACEMENT SECTIONS



DETAIL "A"

REPLACEMENT SECTIONS DRIVING LANES

- 10" JPC

IF SUBGRADE STABILIZATION IS REQUIRED:
- 12" OR DEPTH DIRECTED BY ENGINEER DGA
- FABRIC-GEOTEXTILE TYPE IV

NOT TO SCALE

TYPICAL SECTIONS

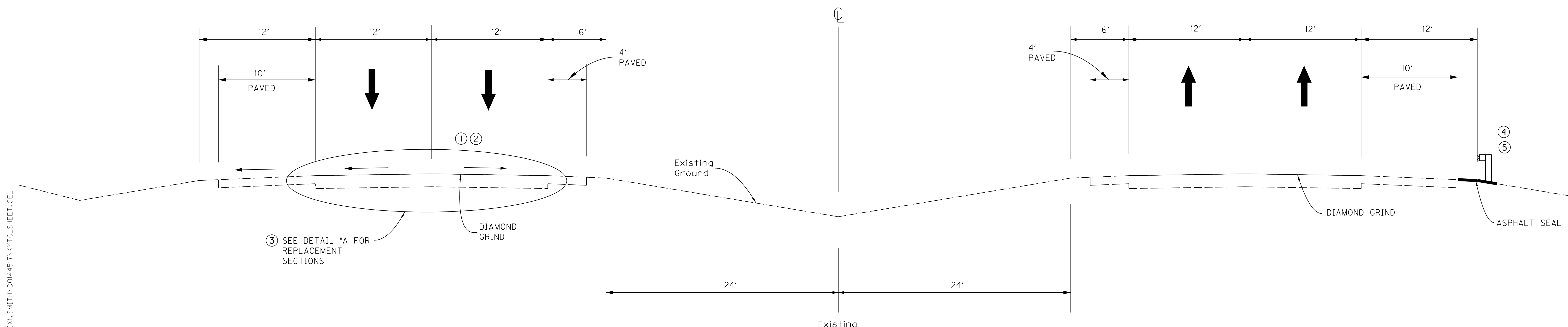
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USER: Alexi.Smith
DATE PLOTTED: January 1, 2001

E-SHEET NAME:

MicroStation v8.11.7.180

I-24 / US 41A INTERCHANGE I-24 TYPICAL SECTIONS



NORMAL SECTION – I-24

STA. 5709+38 TO STA. 5731+32

I-24 NOTES

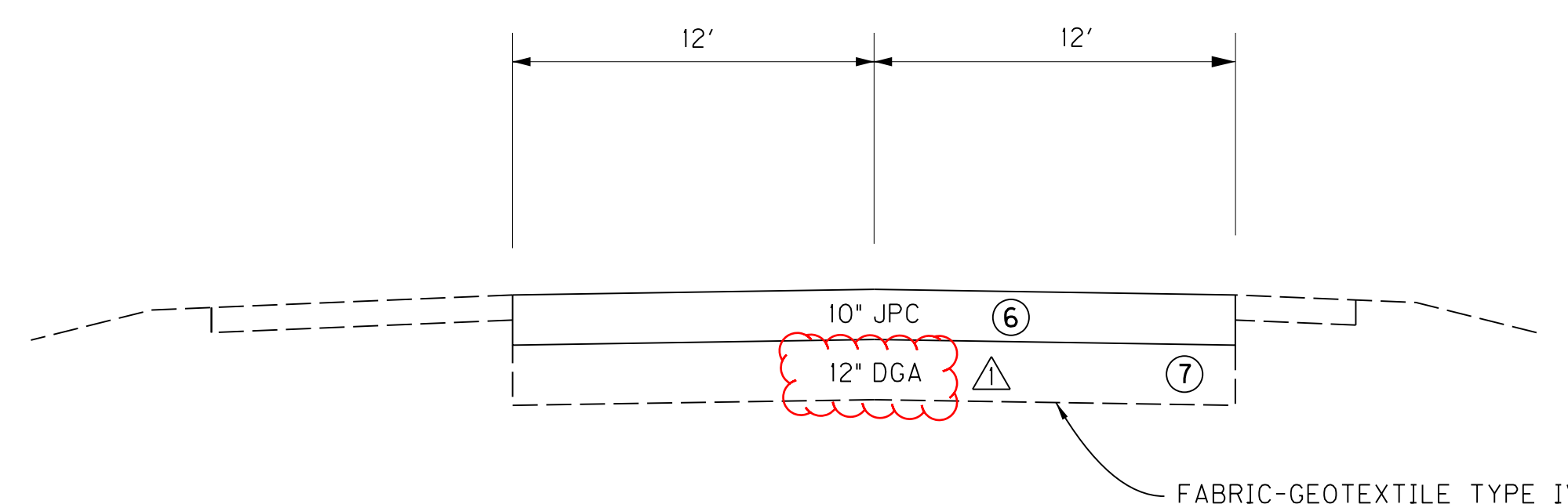
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PAVEMENT REHABILITATION RIGID PAVEMENT – DRIVING LANES

- DIAMOND GRIND EXISTING SURFACE

REPLACEMENT SECTIONS



DETAIL "A"

REPLACEMENT SECTIONS DRIVING LANES

- 10" JPC

IF SUBGRADE STABILIZATION IS REQUIRED:
- 12" OR DEPTH DIRECTED BY ENGINEER DGA △
- FABRIC-GEOTEXTILE TYPE IV

NOT TO SCALE

TYPICAL SECTIONS

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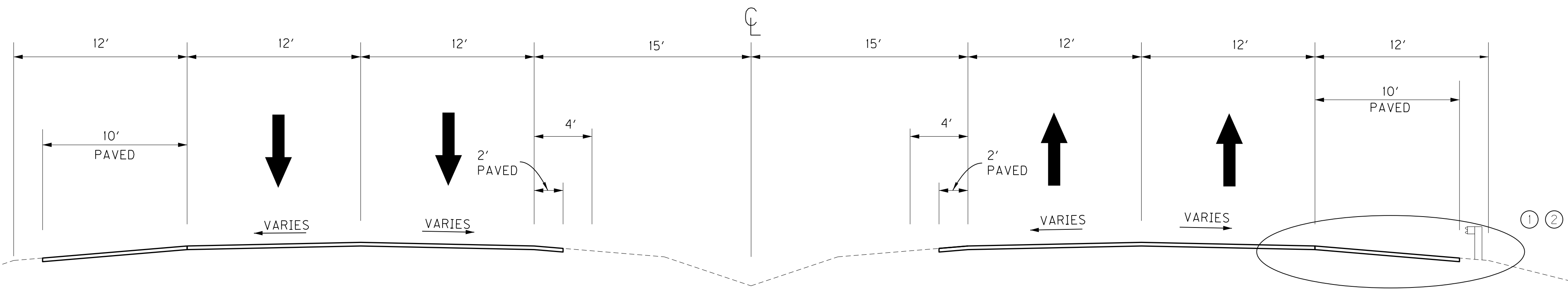
USER: Alexi.Smith
DATE PLOTTED: January 1, 0001

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MicroStation v8.11.7.180

I-24 /US 41A INTERCHANGE

US 41A TYPICAL SECTIONS



US 41A NOTES

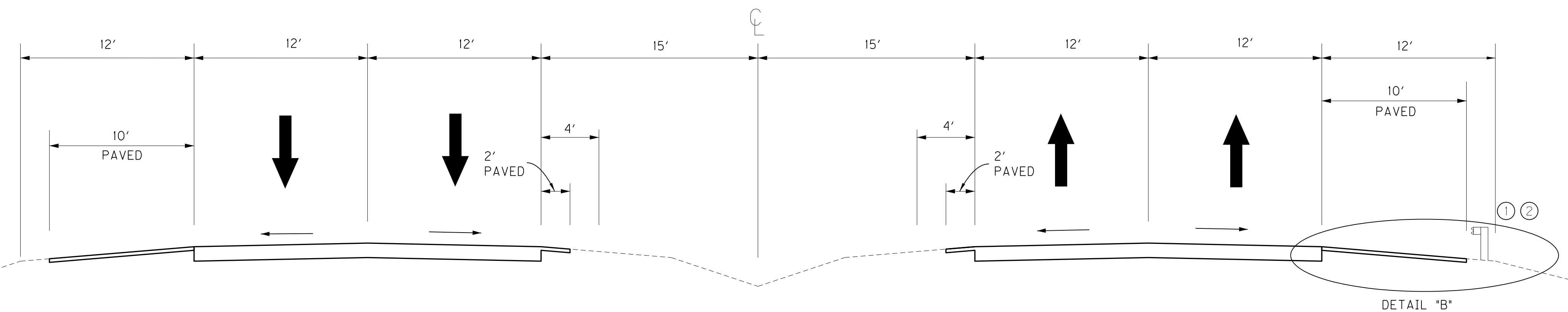
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NORMAL SECTION - US 41A

STA. 84+50 TO STA. 104+45 NORTHBOUND
STA. 107+60 TO STA. 114+00 NORTHBOUND
STA. 84+50 TO STA. 103+90 SOUTHBOUND
STA. 107+15 TO STA. 114+00 SOUTHBOUND

PAVEMENT REHABILITATION FLEXIBLE PAVEMENT - DRIVING LANES & SHOULDERS

SURFACE - 1.5" CL4 ASPH SURF 0.5A (PG64-22)
1.5" ASPHALT PAVE MILLING & TEXTURING



NORMAL SECTION - US 41A

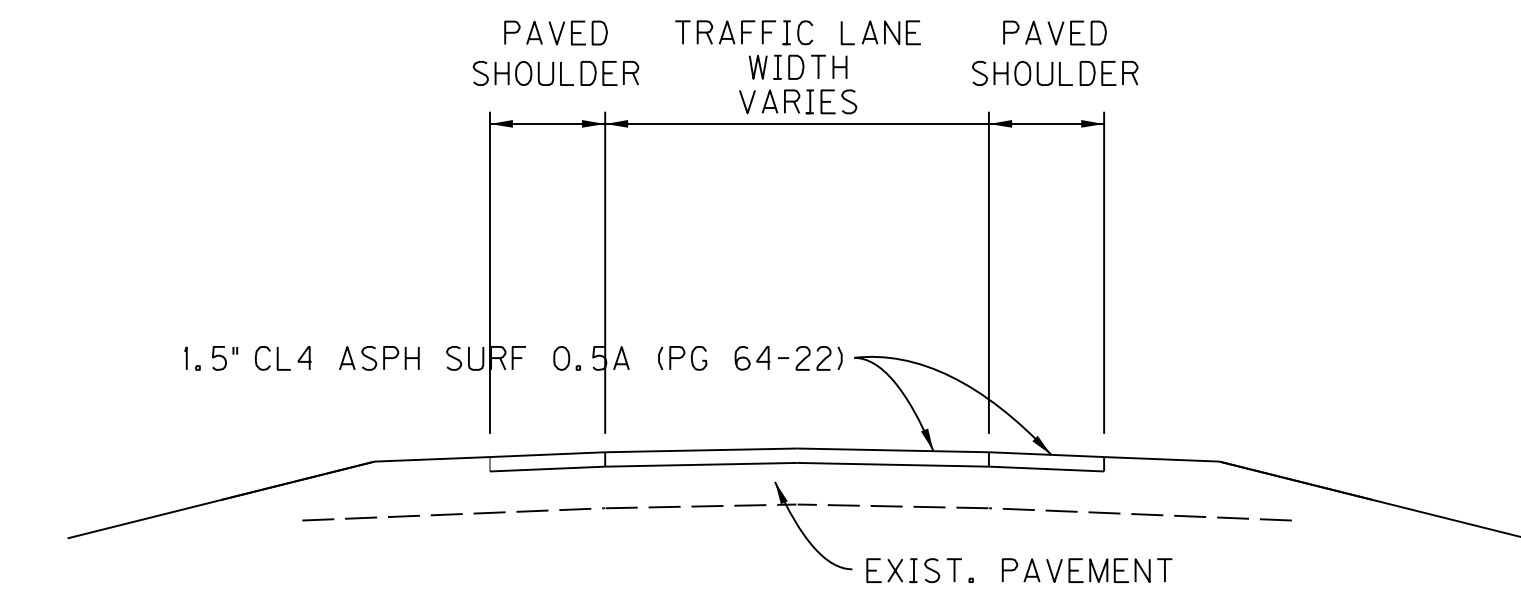
STA. 104+45 TO STA. 107+60 NORTHBOUND
STA. 103+90 TO STA. 107+15 SOUTHBOUND

PAVEMENT REHABILITATION RIGID PAVEMENT - DRIVING LANES

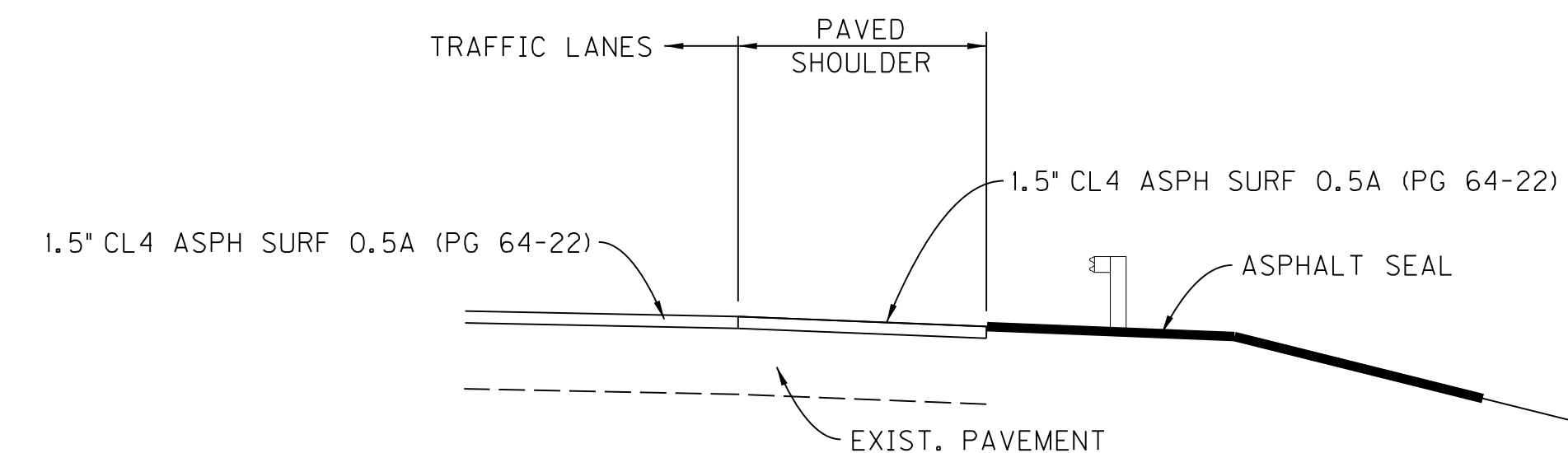
SURFACE - 6" JPC PAVEMENT
6" ASPHALT PAVE MILLING & TEXTURING

FLEXIBLE PAVEMENT - SHOULDERS

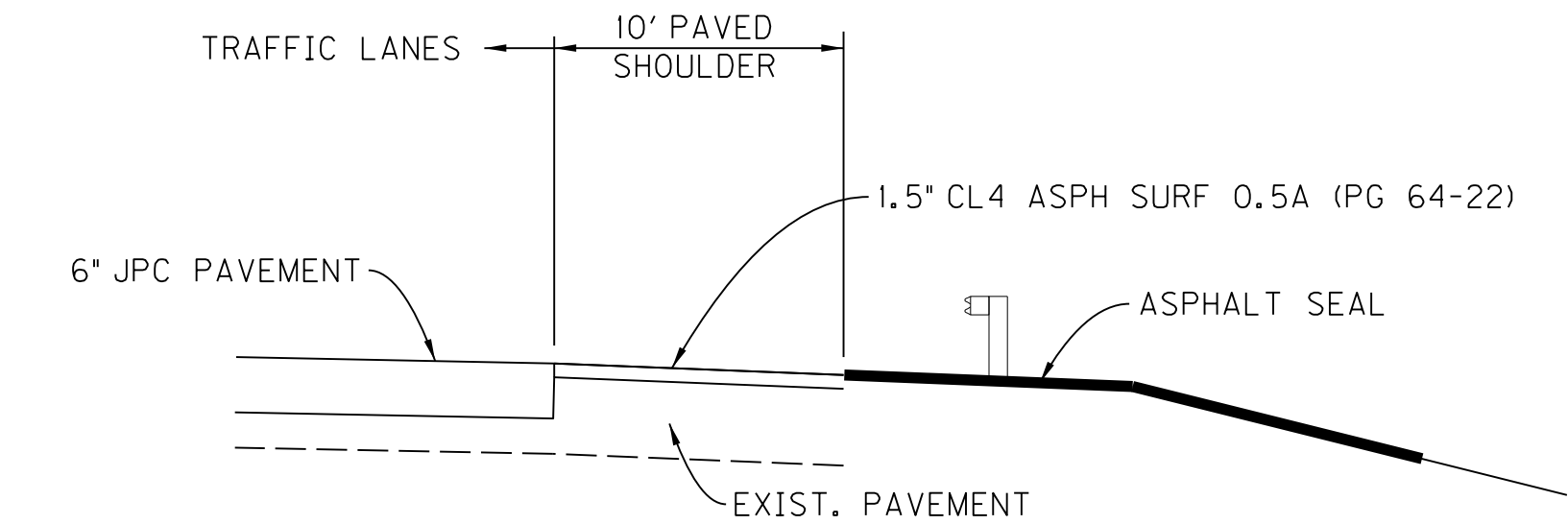
SURFACE - 1.5" CL4 ASPH SURF 0.5A (PG64-22)
1.5" ASPHALT PAVE MILLING & TEXTURING



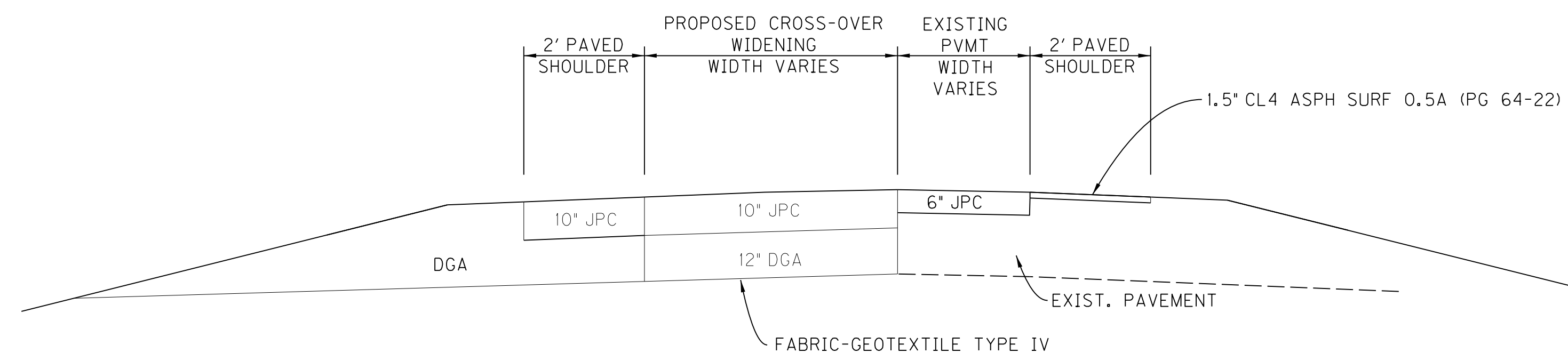
MEDIAN CROSS-OVER US 41A AT RAMP C



DETAIL "A"



DETAIL "B"



MEDIAN CROSS-OVER US 41A AT RAMP D

NOT TO SCALE

TYPICAL SECTIONS

FILE NAME: G:\PWORK\VALEXI.SMITH\00144517\KYTC-SHEET.CEL

USER: AlexL.Smith
DATE PLOTTED: January 1, 0001

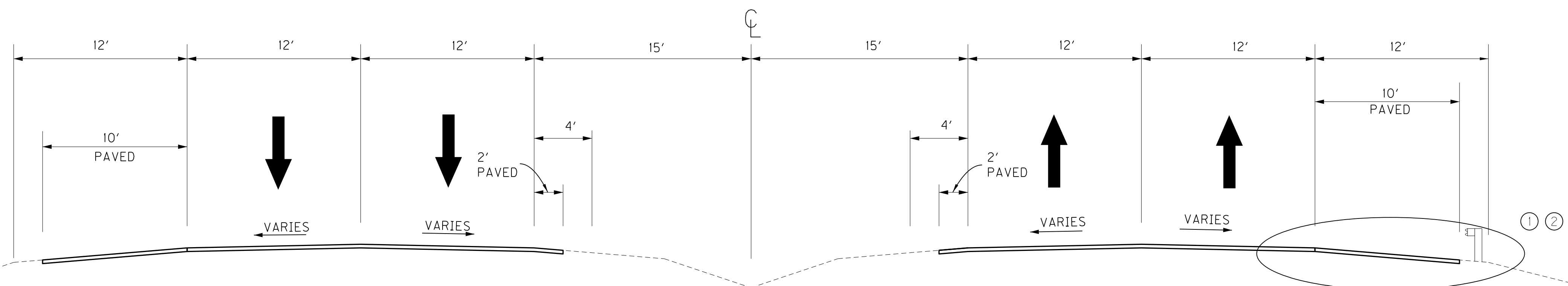
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REVISD 5-3-11

I-24 /US 41A INTERCHANGE

US 41A TYPICAL SECTIONS



US 41A NOTES

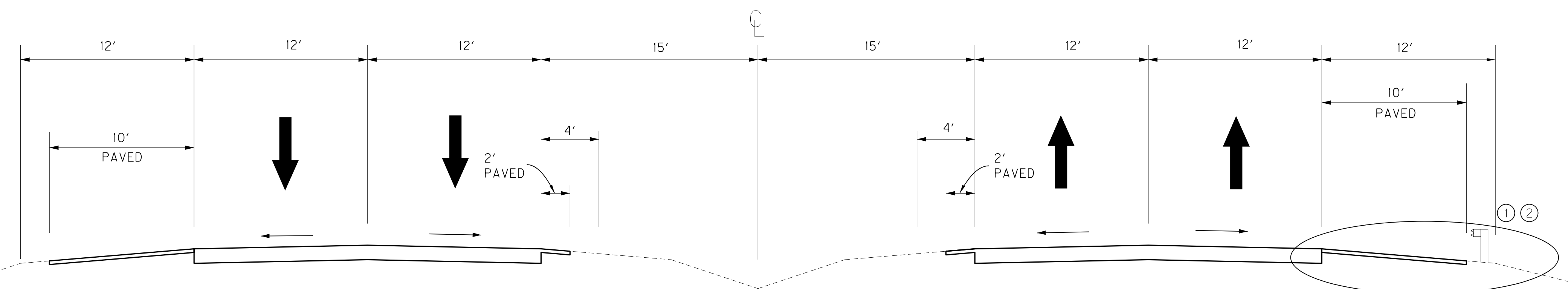
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STA. 107+15 TO STA. 114+00 SOUTHBOUND

PAVEMENT REHABILITATION FLEXIBLE PAVEMENT - DRIVING LANES & SHOULDERS

SURFACE - 1.5" CL4 ASPH SURF 0.5A (PG64-22)
1.5" ASPHALT PAVE MILLING & TEXTURING



NORMAL SECTION - US 41A

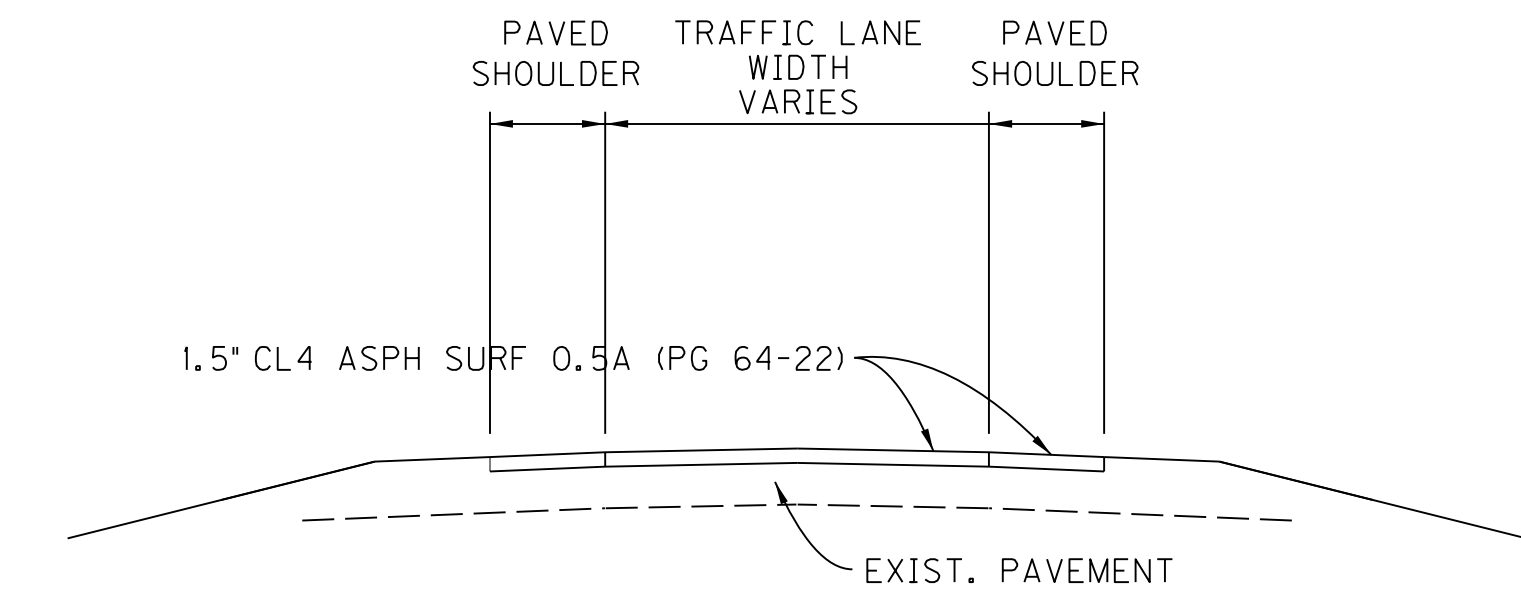
STA. 104+45 TO STA. 107+60 NORTHBOUND
STA. 103+90 TO STA. 107+15 SOUTHBOUND

PAVEMENT REHABILITATION RIGID PAVEMENT - DRIVING LANES

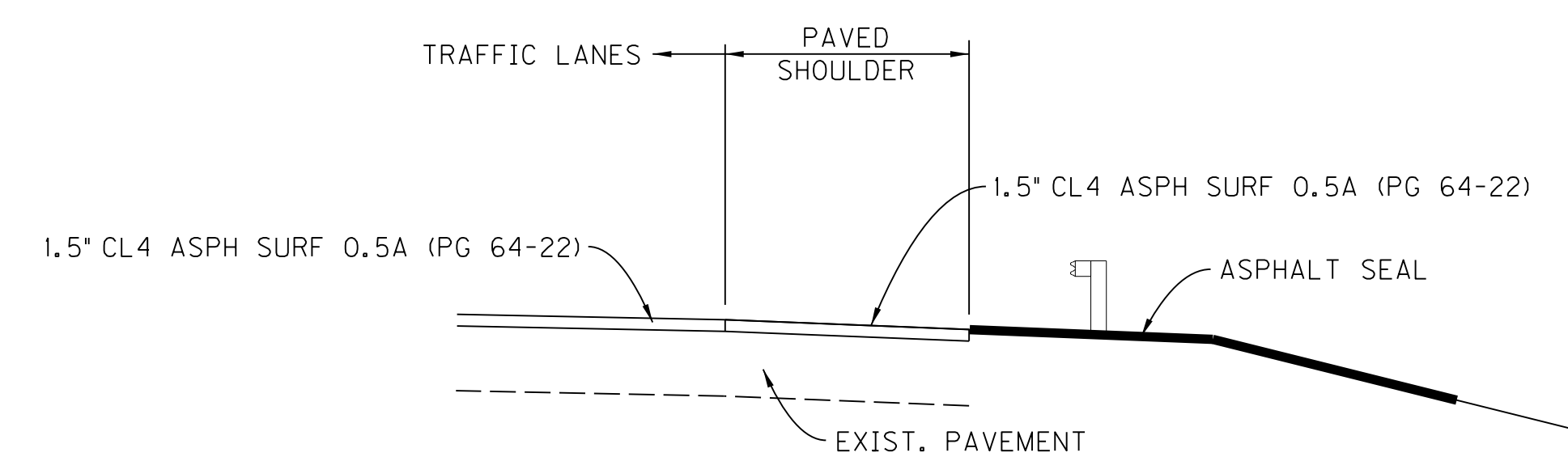
SURFACE - 6" JPC PAVEMENT
6" ASPHALT PAVE MILLING & TEXTURING

FLEXIBLE PAVEMENT - SHOULDERS

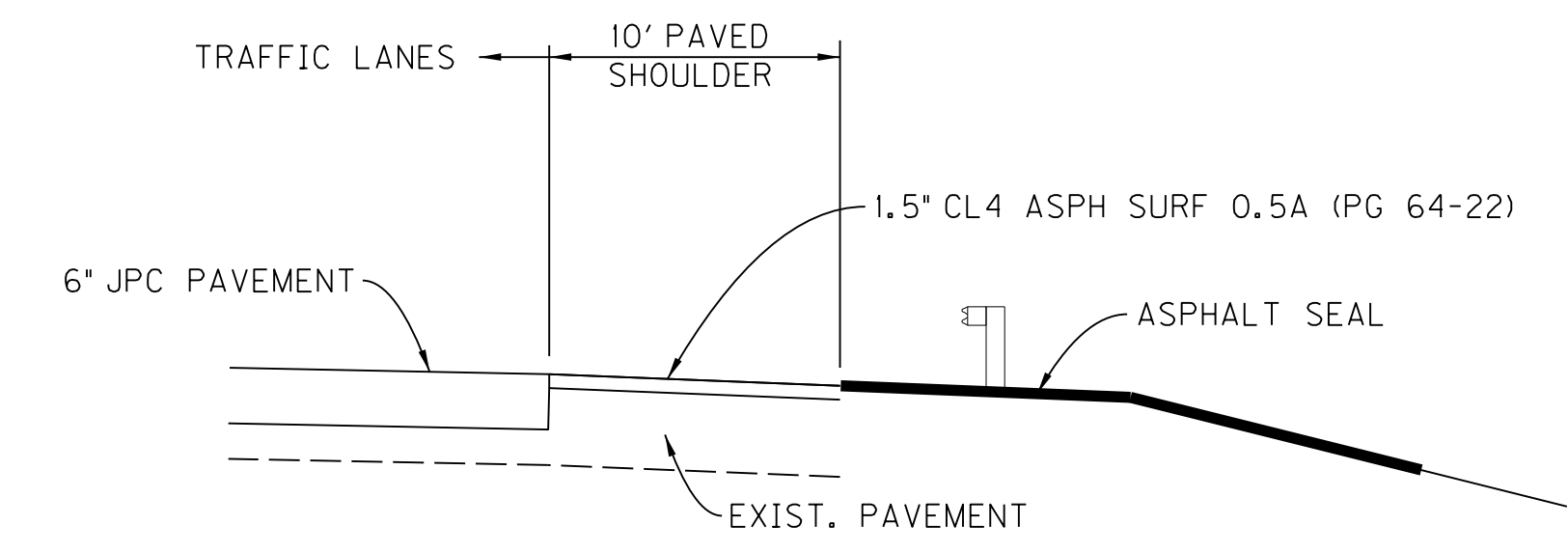
SURFACE - 1.5" CL4 ASPH SURF 0.5A (PG64-22)
1.5" ASPHALT PAVE MILLING & TEXTURING



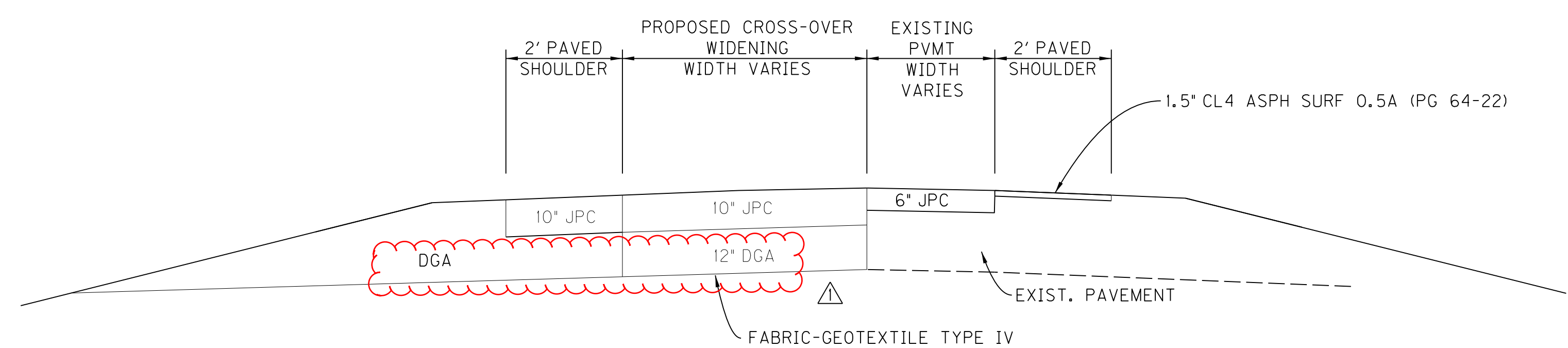
MEDIAN CROSS-OVER US 41A AT RAMP C



DETAIL "A"



DETAIL "B"



MEDIAN CROSS-OVER US 41A AT RAMP D

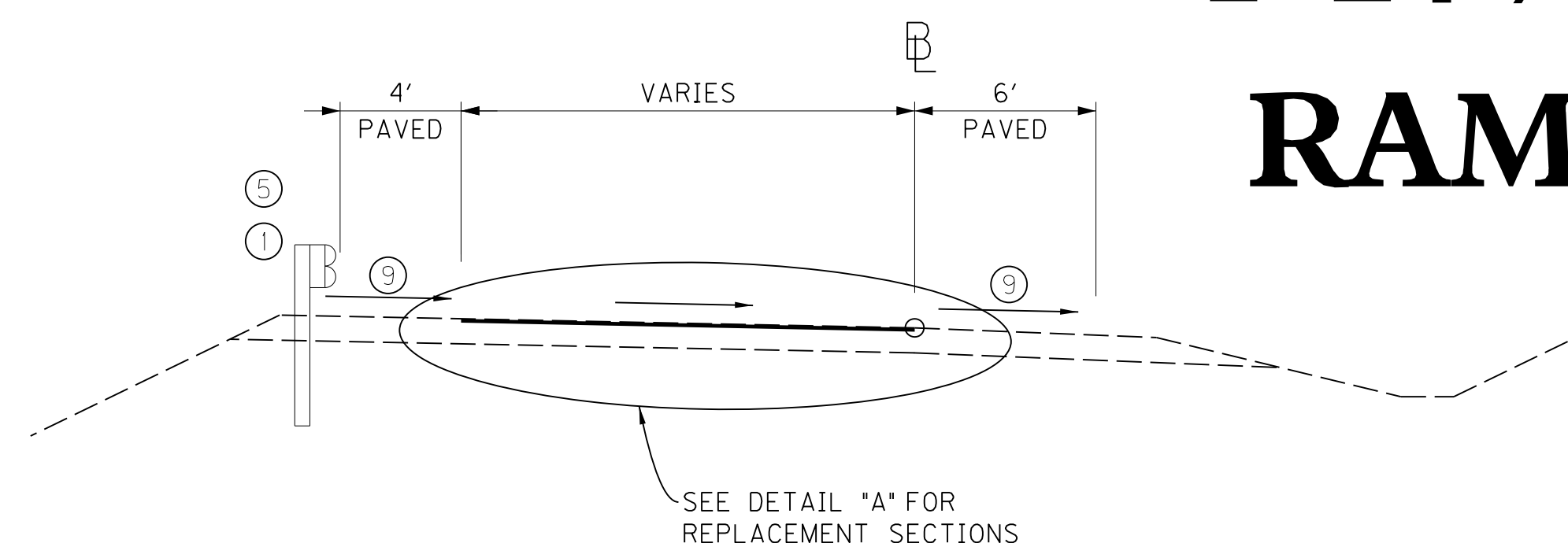
NOT TO SCALE

TYPICAL SECTIONS

FILE NAME: G:\PWORK\VALEXI.SMITH\00144517\KYTC-SHEET.CEL
USER: Alexi.Smith
DATE PLOTTED: January 1, 2001
E-SHEET NAME:
MicroStation v8.11.7.180

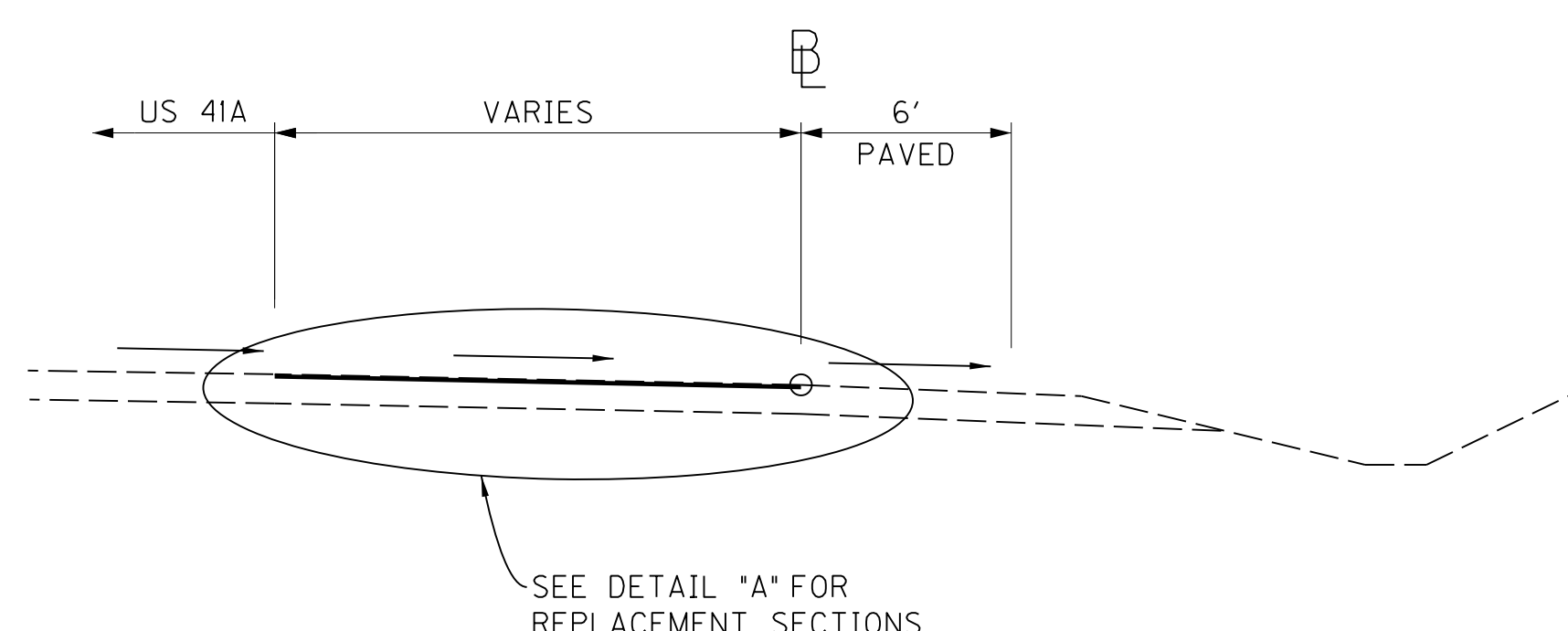
I-24 /US 41A INTERCHANGE

RAMP TYPICAL SECTIONS



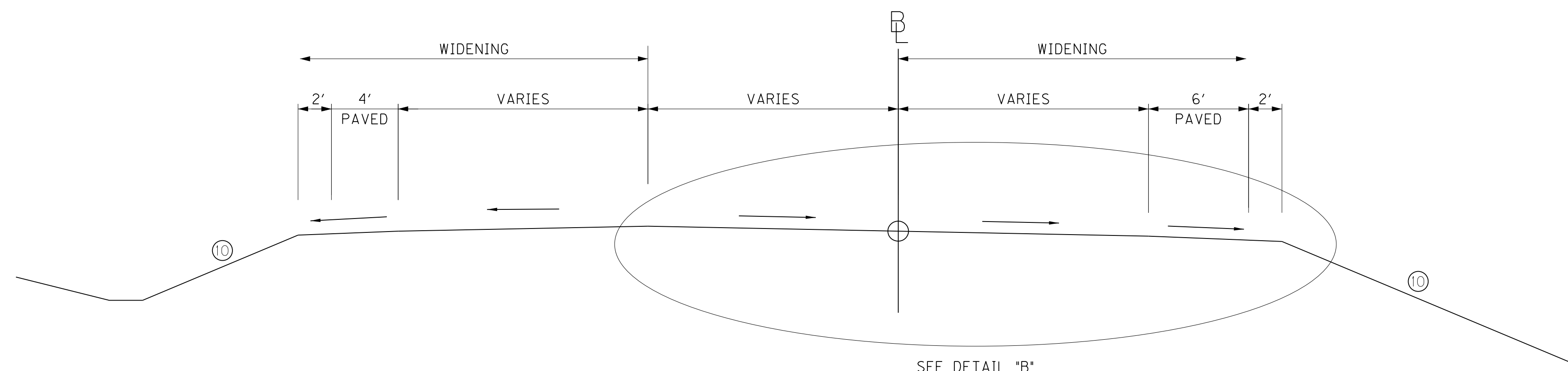
DIAMOND GRIND TYPICAL

RAMPS A, B, C, E & F
RAMP D STA. 0+00 TO 21+75



DIAMOND GRIND TYPICAL

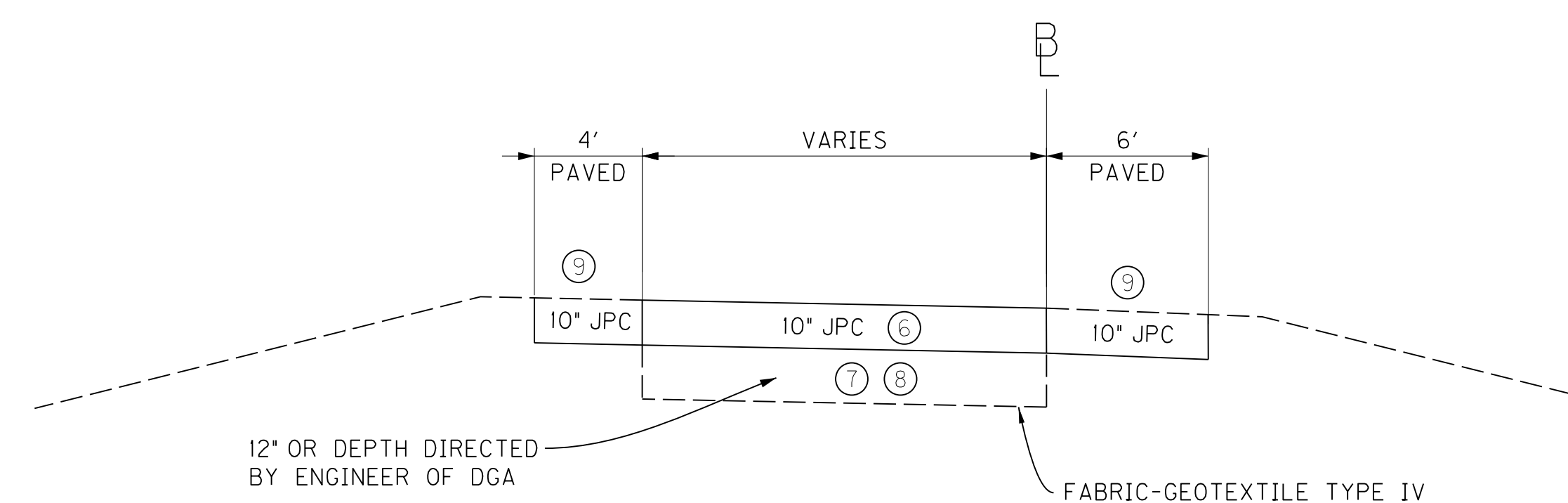
RAMP D1 STA. 4+00 TO 9+37



WIDENING & REPLACEMENT TYPICAL

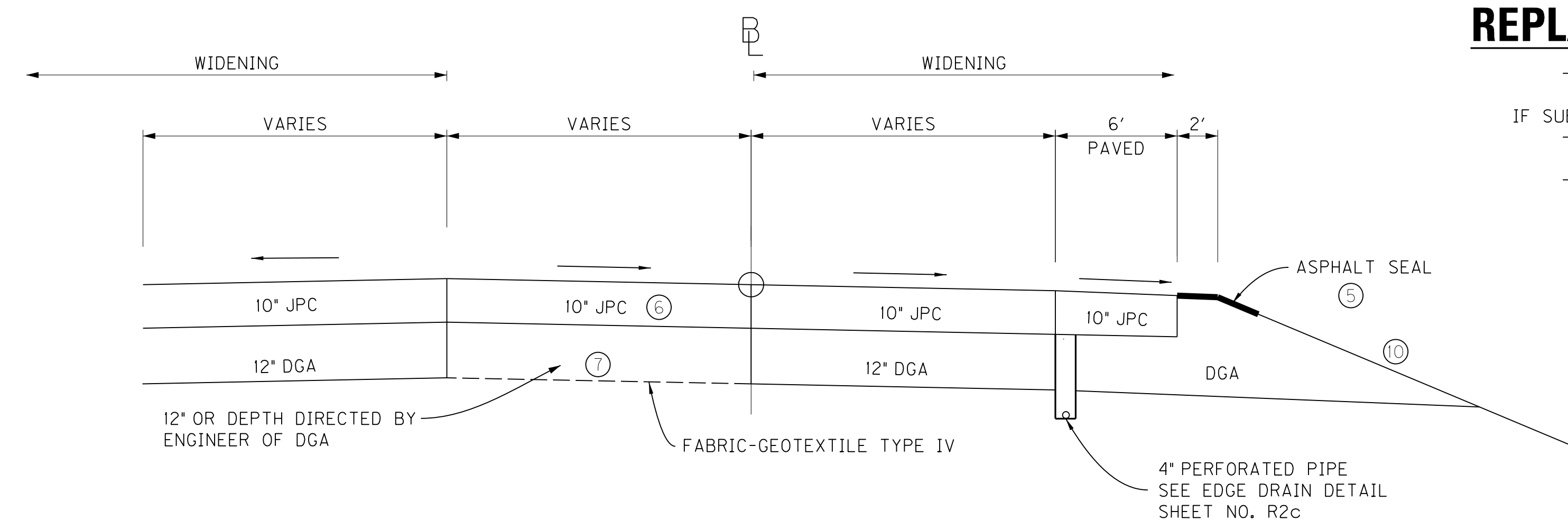
RAMP D STA. 21+75 TO 27+01
RAMP D1 0+00 TO 4+00

SEE RAMP D/ RAMP D1/ US 41A DEVELOPMENT SHEETS
FOR CROSS-SLOPES - SHEET NOS. R9 - R15.



DETAIL "A"

RAMPS A, B, C, E, & F
RAMP D STA. 0+00 TO 21+75



DETAIL "B"

RAMP NOTES

- ① REMOVE EXISTING GUARDRAIL AND REPLACE. NEW GUARDRAIL TO HAVE 7' POST. SEE GUARDRAIL SUMMARY SHEET FOR LOCATIONS.
- ② ALL JOINTS TO BE RESEALED.
- ③ PAVEMENT REPLACEMENT LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER AND ARE TO BE IN ACCORDANCE WITH THE PROVIDED DETAILS AND STANDARD SPECIFICATIONS, FOR ALL RAMPS EXCEPT RAMP D FROM STA. 16+00 TO 21+75.
- ④ DRIVING LANES TO BE DIAMOND GROUND.
- ⑤ FOR GUARDRAIL REPLACEMENT AREAS, TOTAL LENGTH OF SHOULDER ON RAMP D 16+00 TO END AND RAMP D1 0+00 TO 4+00: ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE. TWO APPLICATIONS OF THE FOLLOWING:
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ASPHALT SEAL AGGREGATE 20 LBS/SY
- ⑥ THROUGH THE LIMITS OF THE REPLACEMENT SECTIONS, AS DETERMINED BY NOTE 3, EXISTING CONCRETE PAVEMENT TO BE REMOVED AND REPLACED WITH 10" JPC.
- ⑦ IF UPON EXCAVATION OF THE EXISTING CONCRETE PAVEMENT THE RESIDENT ENGINEER DETERMINES THE SUBGRADE TO BE UNSUITABLE, THE CONTRACTOR WILL EXCAVATE 12", OR DEPTH DIRECTED BY ENGINEER, AND CONSTRUCT EQUIVALENT DEPTH OF DGA OVER FABRIC-GEOTEXTILE TYPE IV.
- ⑧ FOR RAMP D STA. 16+00 TO 21+75, ALL EXISTING CONCRETE TO BE REMOVED AND REPLACED WITH 10" JPC, INCLUDING SHOULDERS. IF SUBGRADE IS UNSUITABLE, EXCAVATE 12", OR DEPTH DIRECTED BY ENGINEER, AND CONSTRUCT EQUIVALENT DEPTH OF DGA OVER FABRIC-GEOTEXTILE TYPE IV.
- ⑨ SHOULDER REPLACED ON RAMP D STA. 16+00 TO 21+75 ONLY.
- ⑩ SEE RAMP D & RAMP D1 CROSS-SECTIONS FOR SIDE SLOPES.

PAVEMENT REHABILITATION RIGID PAVEMENT – DRIVING LANES

- DIAMOND GRIND EXISTING SURFACE

REPLACEMENT SECTIONS

- 10" JPC

IF SUBGRADE STABILIZATION IS REQUIRED:
- 12" OR DEPTH DIRECTED BY ENGINEER OF DGA
- FABRIC-GEOTEXTILE TYPE IV

NOT TO SCALE

TYPICAL SECTIONS

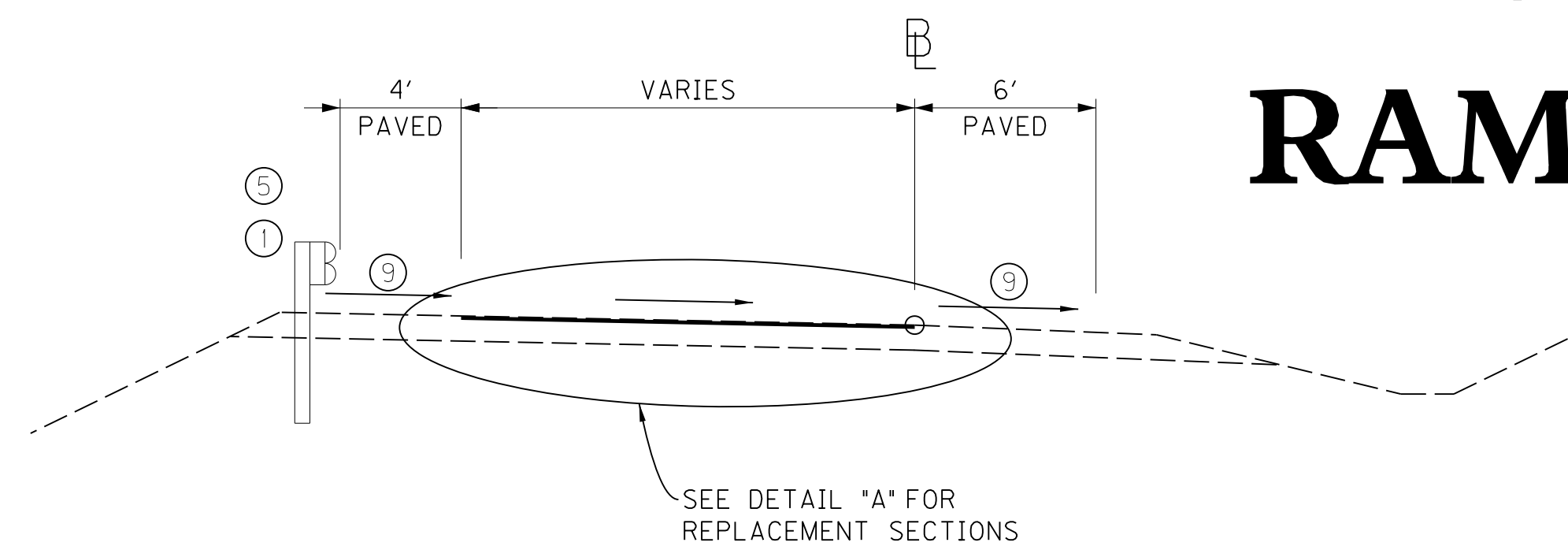
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USER: Alexi.Smith
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E-SHEET NAME:

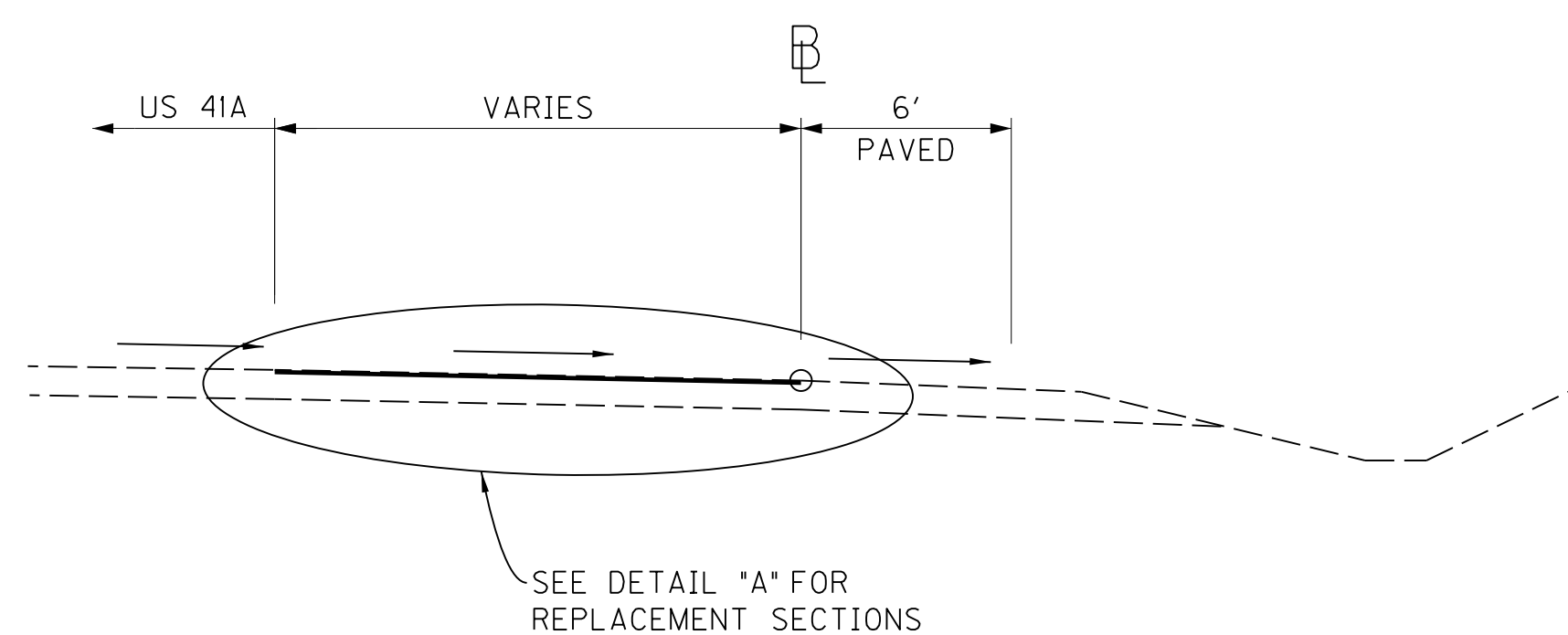
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I-24 /US 41A INTERCHANGE RAMP TYPICAL SECTIONS



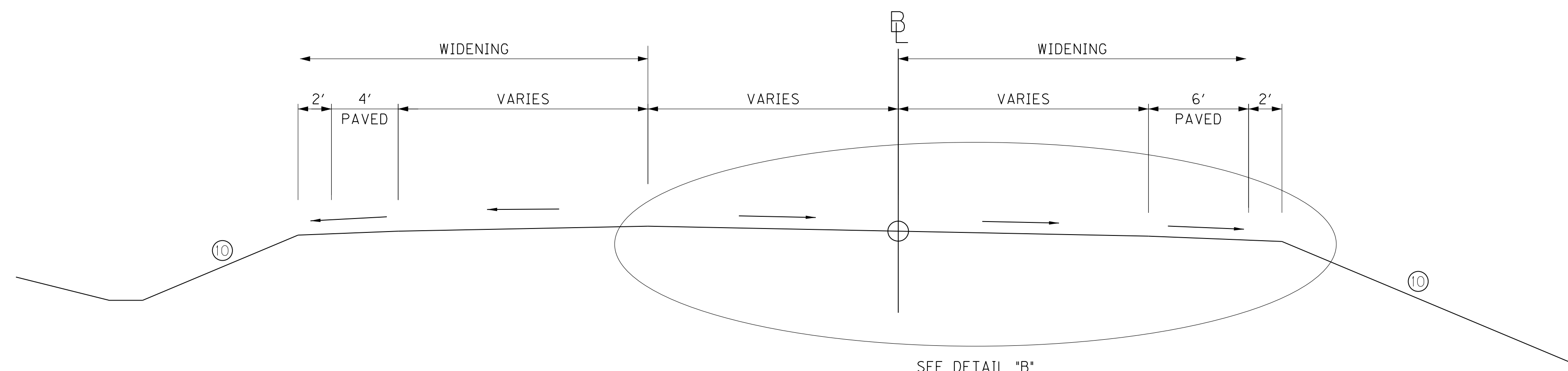
DIAMOND GRIND TYPICAL

RAMPS A, B, C, E & F
RAMP D STA. 0+00 TO 21+75



DIAMOND GRIND TYPICAL

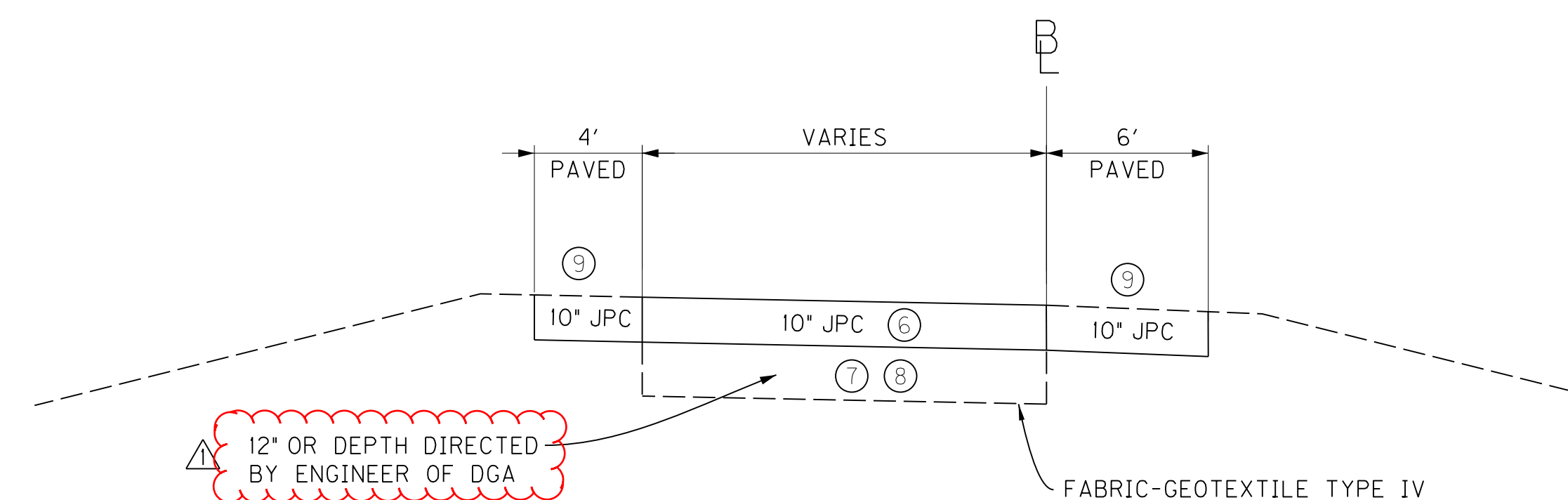
RAMP D1 STA. 4+00 TO 9+37



WIDENING & REPLACEMENT TYPICAL

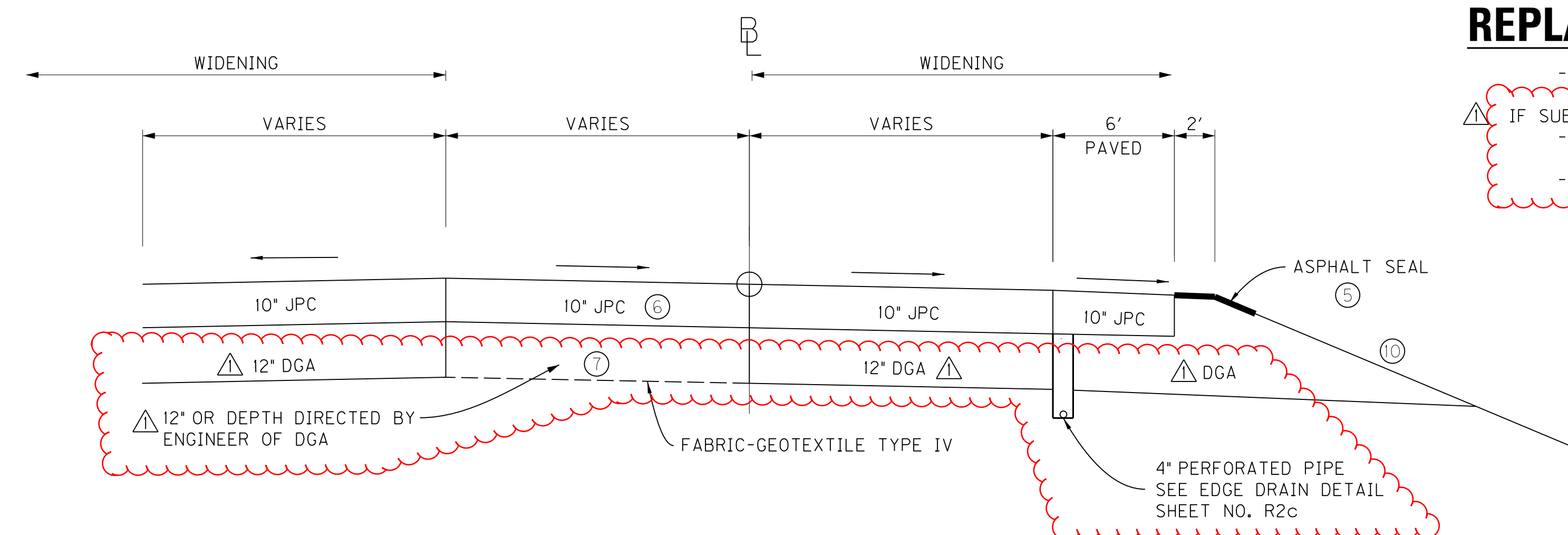
RAMP D STA. 21+75 TO 27+01
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SEE RAMP D/ RAMP D1/ US 41A DEVELOPMENT SHEETS
FOR CROSS-SLOPES - SHEET NOS. R9 - R15.



DETAIL "A"

RAMPS A, B, C, E, & F
RAMP D STA. 0+00 TO 21+75



DETAIL "B"

NOT TO SCALE

PAVEMENT REHABILITATION RIGID PAVEMENT – DRIVING LANES

- DIAMOND GRIND EXISTING SURFACE

REPLACEMENT SECTIONS

- 10" JPC
- △ IF SUBGRADE STABILIZATION IS REQUIRED:
 - 12" OR DEPTH DIRECTED BY ENGINEER OF DGA
 - FABRIC-GEOTEXTILE TYPE IV

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- SHOULDER REPLACED ON RAMP D STA. 16+00 TO 21+75 ONLY.
- SEE RAMP D & RAMP D1 CROSS-SECTIONS FOR SIDE SLOPES.

TYPICAL SECTIONS

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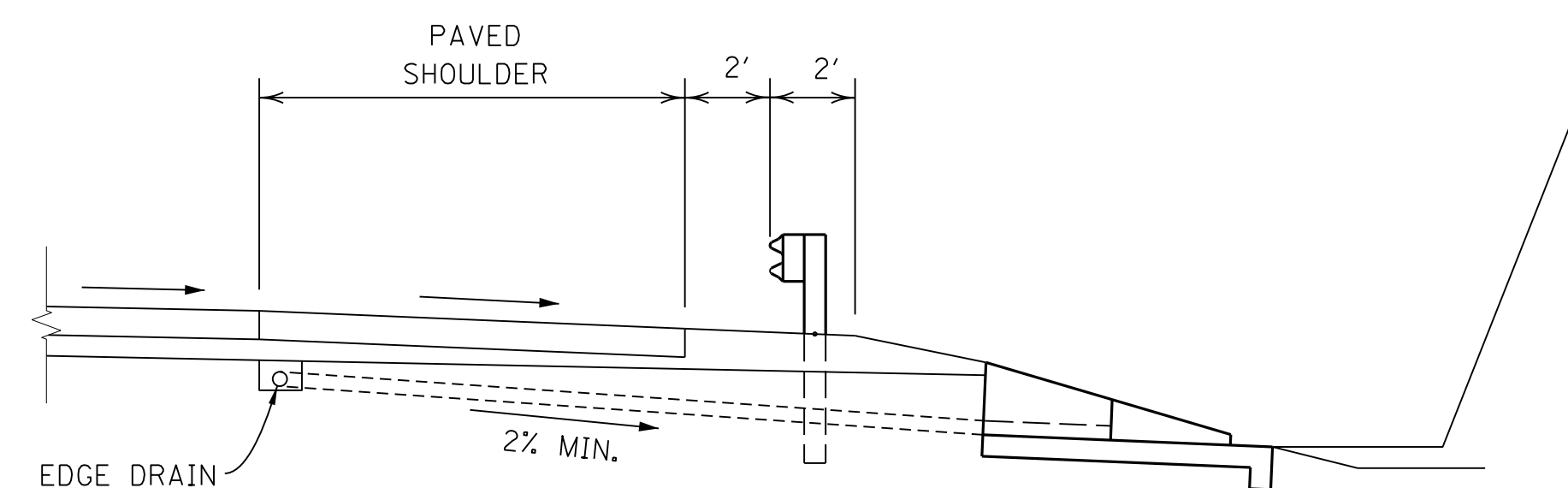
E-SHEET NAME:

MicroStation v8.11.7.180

COUNTY OF	ITEM NO.	SHEET NO.
CHRISTIAN	2-213.00	R2c

EDGE DRAIN TYPICAL CHANNEL LINING TYPICAL

LONGITUDINAL PAVEMENT EDGE DRAIN (PERFORATED PIPE)



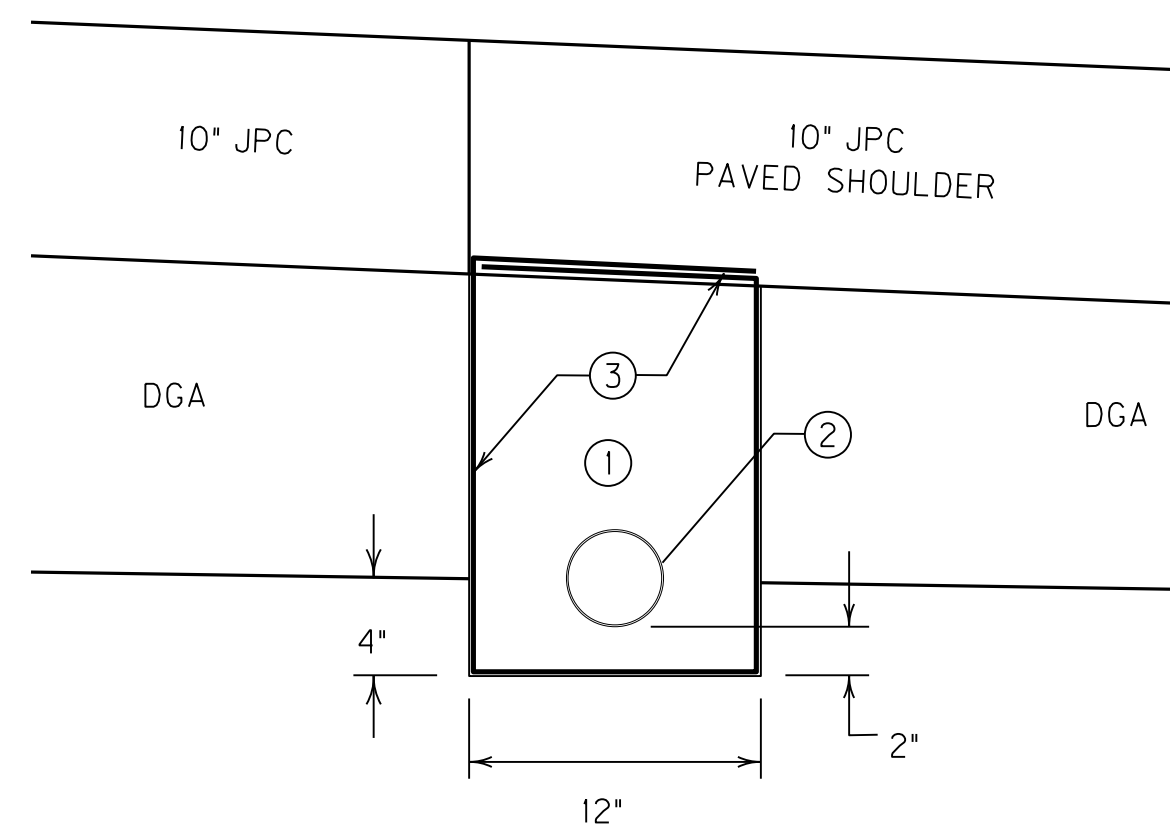
SPECIAL NOTE FOR PAVEMENT SUBSURFACE DRAINAGE OUTLET

A QUANTITY OF CRUSHED AGGREGATE SIZE NO. 2 AS DEFINED IN THE CURRENT "KENTUCKY STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" SHALL BE USED AT ALL PERFORATED PIPE HEADWALL OUTLETS AS ILLUSTRATED IN THE DETAIL AT RIGHT. CRUSHED AGGREGATE SIZE NO. 2 SHALL BE PLACED A MINIMUM DEPTH OF 4 INCHES AS DETAILED AT RIGHT.

DENSE GRADED AGGREGATE (DGA) REMOVED DURING PLACEMENT OF THE CRUSHED AGGREGATE SIZE NO. 2 SHALL BE USED TO DRESS THE EXISTING SHOULDERS WHERE DGA IS EXPOSED. OTHER MATERIALS REMOVED DURING PLACEMENT OF THE CRUSHED AGGREGATE SIZE NO. 2 SHALL BE WASTED AS DIRECTED BY THE ENGINEER. NO DIRECT PAYMENT WILL BE ALLOWED FOR DISPOSAL OF WASTED MATERIAL.

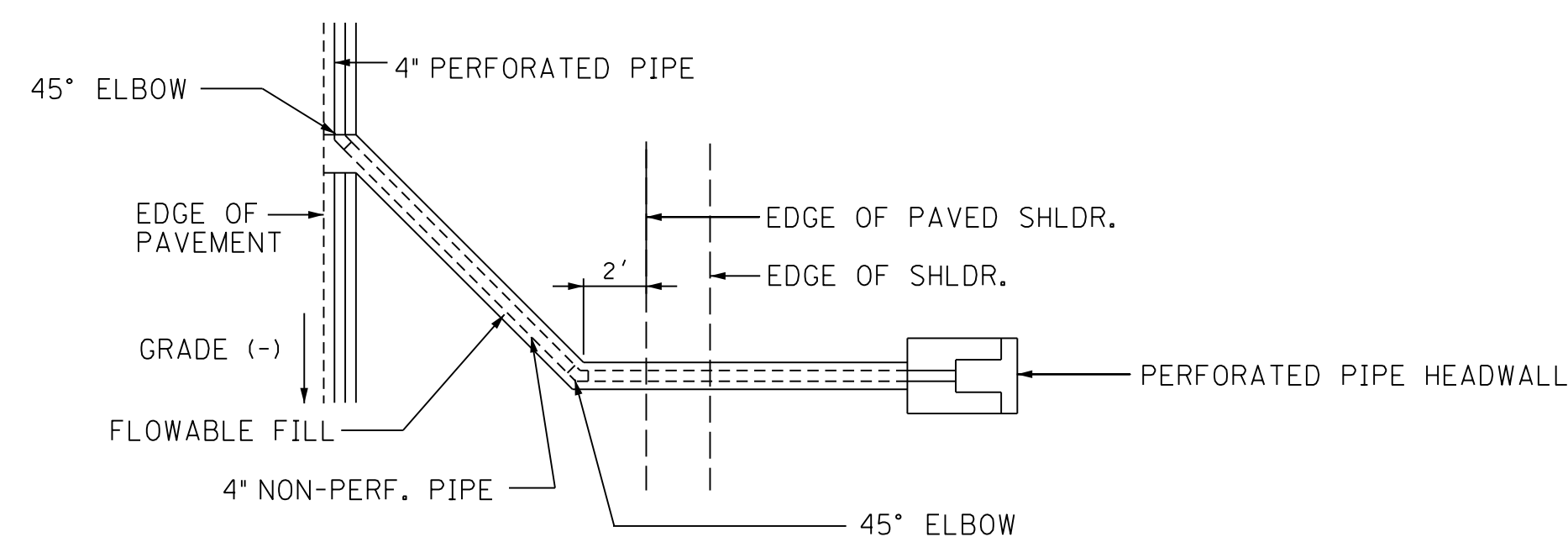
THE CONTRACT UNIT PRICE FOR "CRUSHED AGGREGATE SIZE NO. 2 SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, AND OTHER INCIDENTALS NECESSARY TO PLACE CRUSHED AGGREGATE SIZE NO. 2 FOR CONTROL OF VEGETATION AND/OR EROSION CONTROL AT PAVEMENT EDGE DRAIN OUTLETS.

SEE CURRENT STANDARD DRAWING RDP-010 FOR DIMENSIONS AND OTHER DETAILS. ESTIMATE ONE TON OF CRUSHED AGGREGATE SIZE NO. 2 FOR EACH PERFORATED PIPE HEADWALL OUTLET.

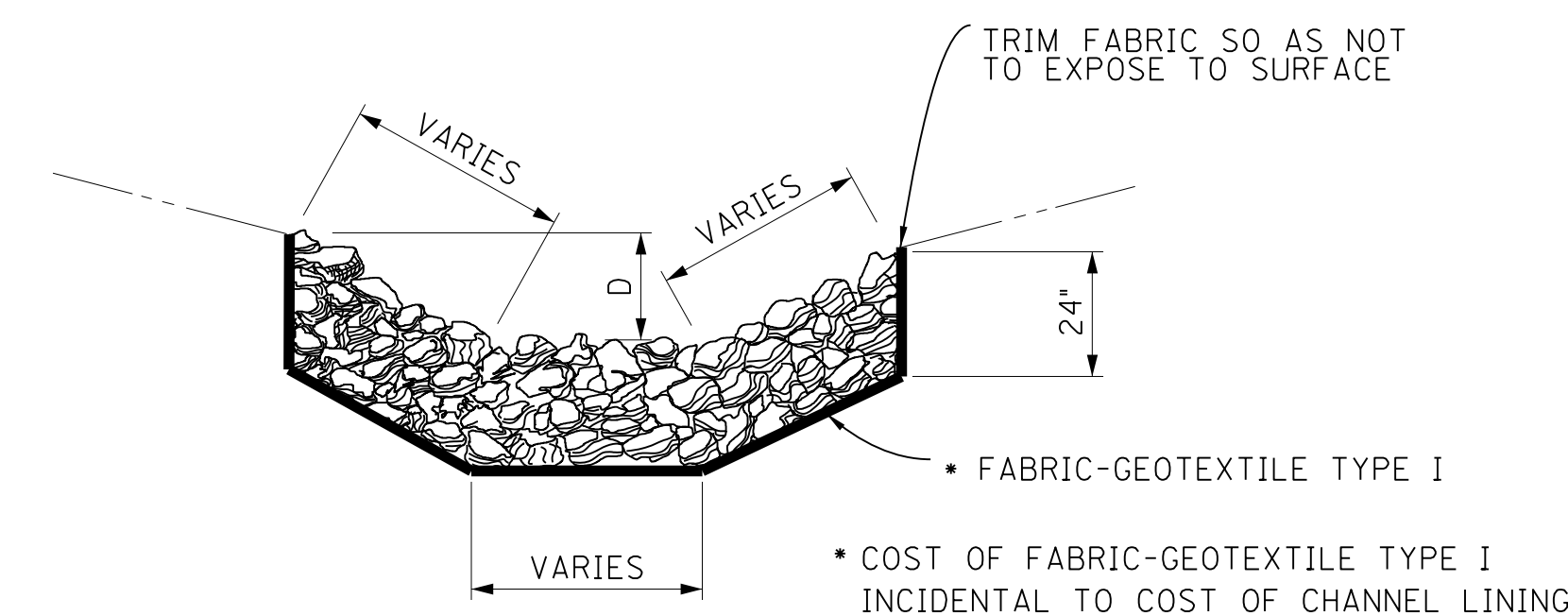


- ① CRUSHED AGGREGATE SIZE No. 57 (NO SAND)
- ② PERFORATED PIPE - 4" (NO SOCK)
NON-PERFORATED PIPE - 4" (OUTLET)
- ③ FABRIC-GEOTEXTILE TYPE IV - DOUBLE OVERLAP ON TOP

PERFORATED PIPE DRAIN

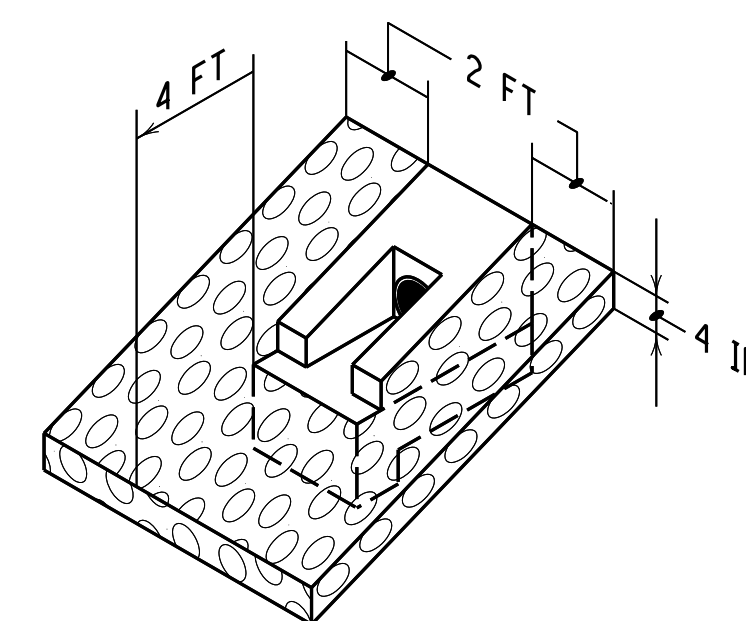


• USE TEE IN LIEU OF ELBOW AT SAG CONDITIONS



CHANNEL LINING TYPICAL SECTION

NOTES
CHANNEL LINING WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR EACH CLASS.
CHANNEL LINING WILL NOT BE REQUIRED IN THE BOTTOM OF THE DITCH WHERE SOLID ROCK IS ENCOUNTERED.
CHANNEL LINING ESTIMATED ON THE BASIS OF 0.50 TON PER SQ. YD. PER FOOT OF DEPTH.
D = DEPTH TO PROTECT



PAVEMENT SUBSURFACE DRAINAGE OUTLET

NOT TO SCALE

DETAILS

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DATE PLOTTED: January 1, 0001

E-SHEET NAME:

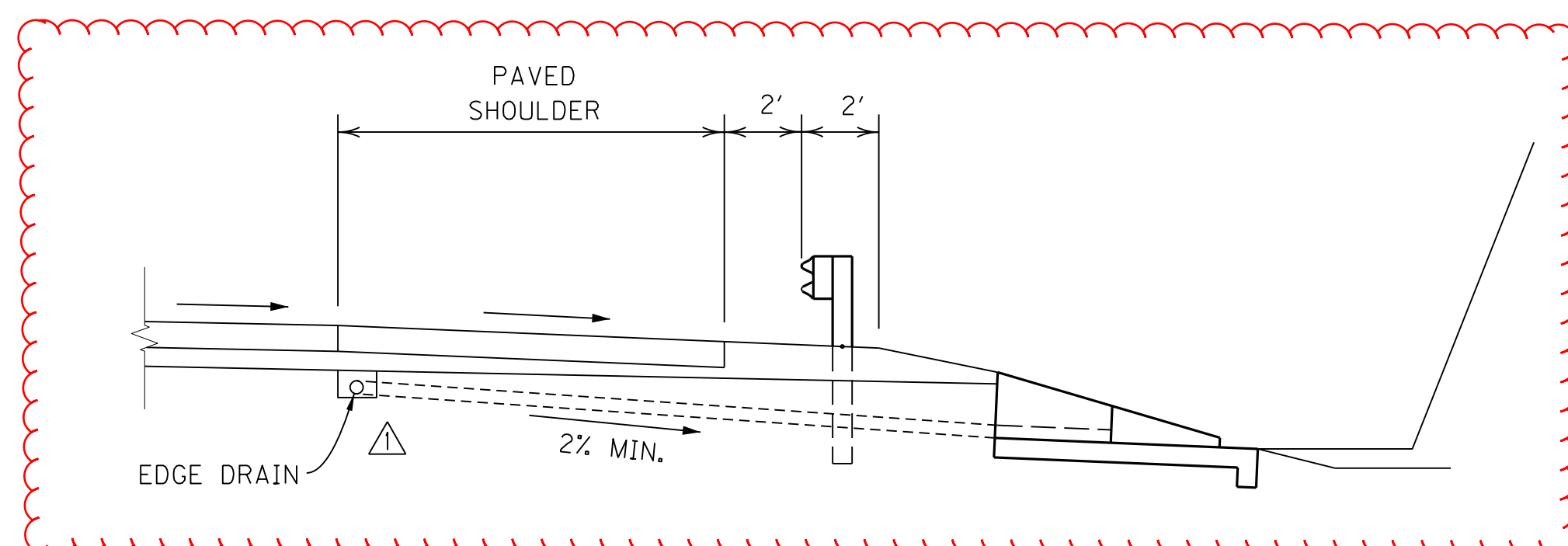
MicroStation v8.11.7.180

COUNTY OF	ITEM NO.	SHEET NO.
CHRISTIAN	2-213.00	R2c

△ REVISED 5-3-11

EDGE DRAIN TYPICAL CHANNEL LINING TYPICAL

LONGITUDINAL PAVEMENT EDGE DRAIN (PERFORATED PIPE)



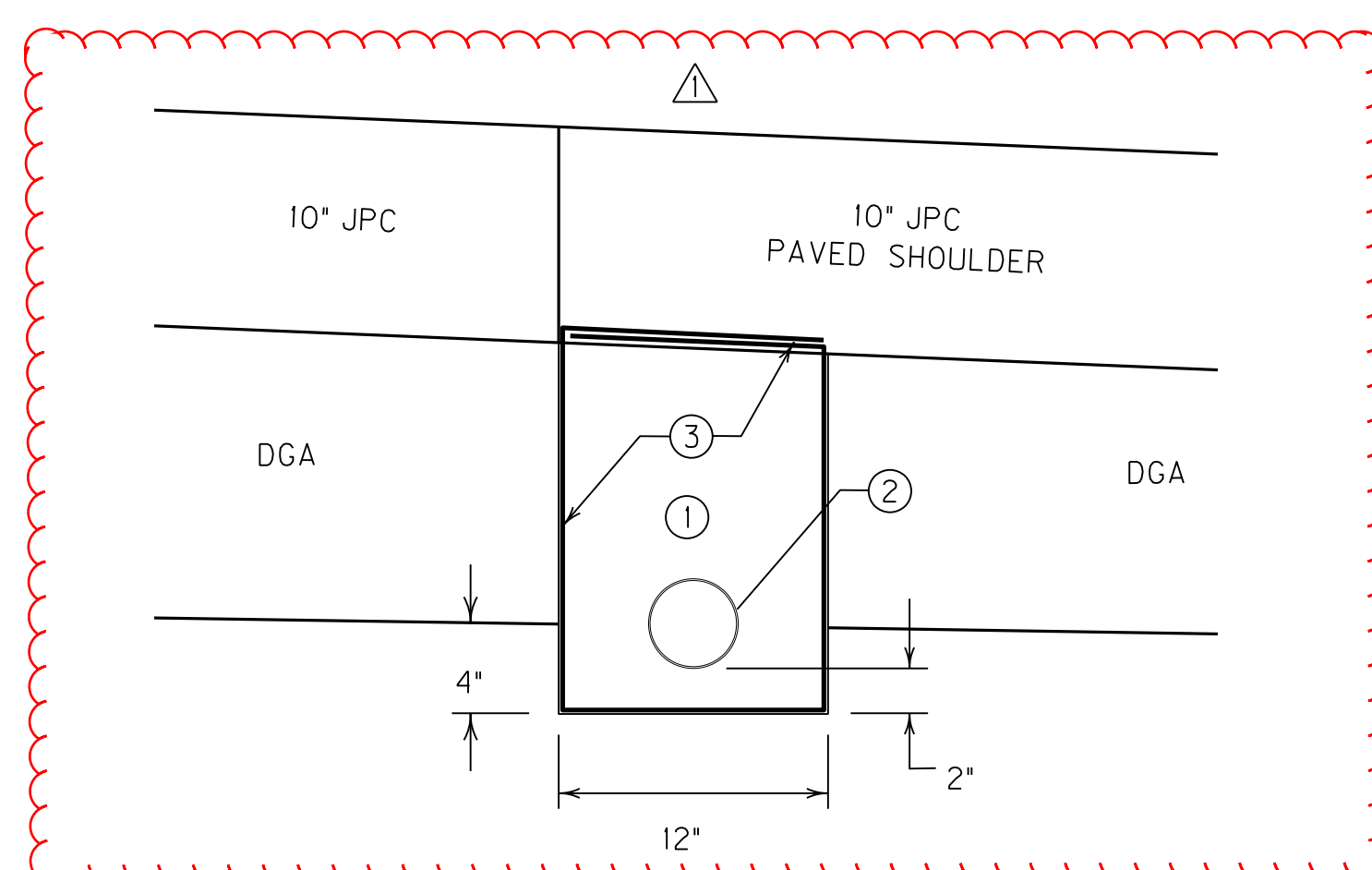
SPECIAL NOTE FOR PAVEMENT SUBSURFACE DRAINAGE OUTLET

A QUANTITY OF CRUSHED AGGREGATE SIZE NO. 2 AS DEFINED IN THE CURRENT "KENTUCKY STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" SHALL BE USED AT ALL PERFORATED PIPE HEADWALL OUTLETS AS ILLUSTRATED IN THE DETAIL AT RIGHT. CRUSHED AGGREGATE SIZE NO. 2 SHALL BE PLACED A MINIMUM DEPTH OF 4 INCHES AS DETAILED AT RIGHT.

DENSE GRADED AGGREGATE (DGA) REMOVED DURING PLACEMENT OF THE CRUSHED AGGREGATE SIZE NO. 2 SHALL BE USED TO DRESS THE EXISTING SHOULDERS WHERE DGA IS EXPOSED. OTHER MATERIALS REMOVED DURING PLACEMENT OF THE CRUSHED AGGREGATE SIZE NO. 2 SHALL BE WASTED AS DIRECTED BY THE ENGINEER. NO DIRECT PAYMENT WILL BE ALLOWED FOR DISPOSAL OF WASTED MATERIAL.

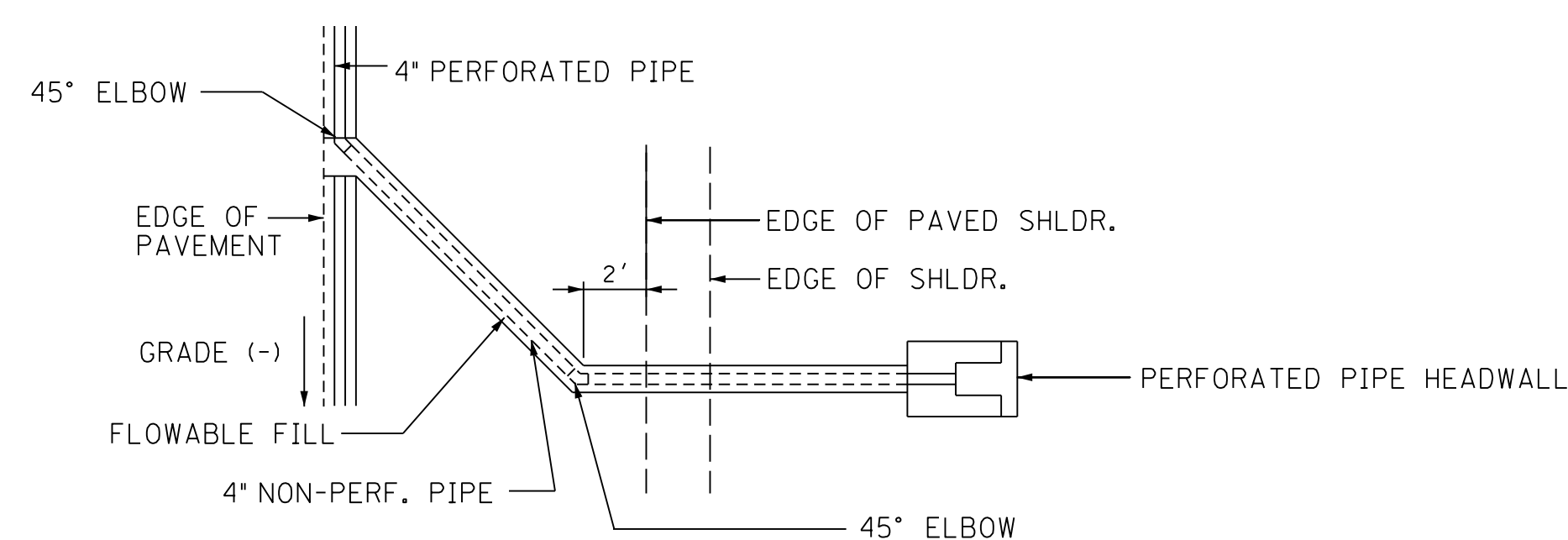
THE CONTRACT UNIT PRICE FOR "CRUSHED AGGREGATE SIZE NO. 2 SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, AND OTHER INCIDENTALS NECESSARY TO PLACE CRUSHED AGGREGATE SIZE NO. 2 FOR CONTROL OF VEGETATION AND/OR EROSION CONTROL AT PAVEMENT EDGE DRAIN OUTLETS.

SEE CURRENT STANDARD DRAWING RDP-010 FOR DIMENSIONS AND OTHER DETAILS. ESTIMATE ONE TON OF CRUSHED AGGREGATE SIZE NO. 2 FOR EACH PERFORATED PIPE HEADWALL OUTLET.

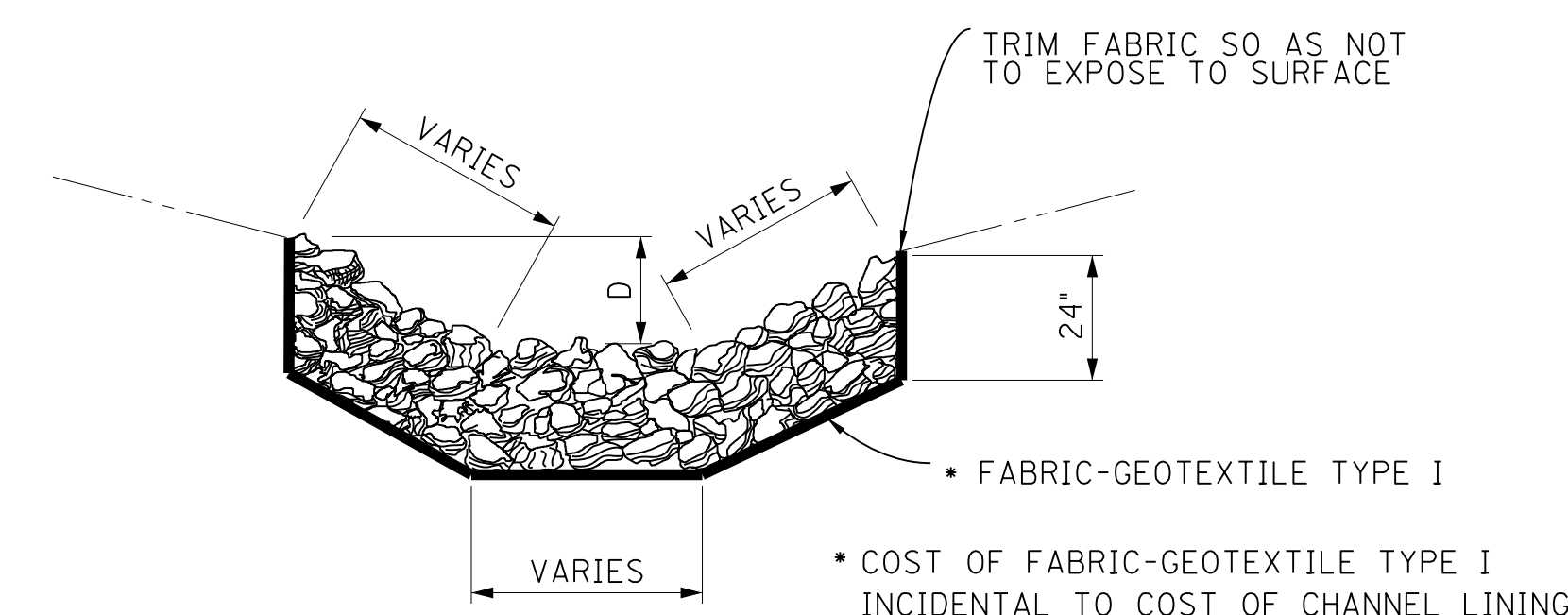


- ① CRUSHED AGGREGATE SIZE No. 57 (NO SAND)
- ② PERFORATED PIPE - 4" (NO SOCK)
NON-PERFORATED PIPE - 4" (OUTLET)
- ③ FABRIC-GEOTEXTILE TYPE IV - DOUBLE OVERLAP ON TOP

PERFORATED PIPE DRAIN

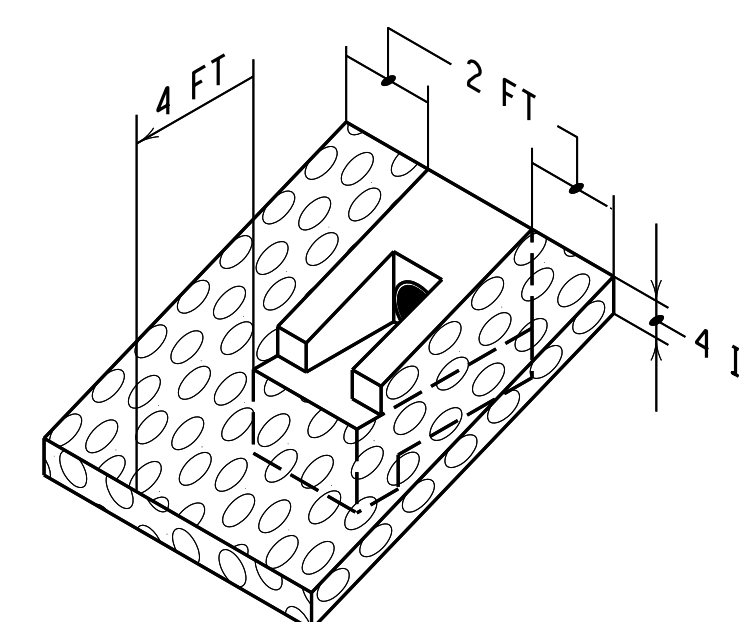


• USE TEE IN LIEU OF ELBOW AT SAG CONDITIONS



CHANNEL LINING TYPICAL SECTION

NOTES
CHANNEL LINING WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR EACH CLASS.
CHANNEL LINING WILL NOT BE REQUIRED IN THE BOTTOM OF THE DITCH WHERE SOLID ROCK IS ENCOUNTERED.
CHANNEL LINING ESTIMATED ON THE BASIS OF 0.50 TON PER SQ. YD. PER FOOT OF DEPTH.
D = DEPTH TO PROTECT



PAVEMENT SUBSURFACE DRAINAGE OUTLET

NOT TO SCALE

DETAILS

FILE NAME: G:\PWORK\VALEXI_S\SMITH\00144517\KYTC-SHEET.CEL

USER: AlexL.Smith
DATE PLOTTED: January 1, 0001

E-SHEET NAME:

MicroStation v8.11.7.180

GENERAL SUMMARY

COUNTY OF	ITEM NO.	SHEET NO.
CHRISTIAN	2-213.00	R2d

ITEM	DESCRIPTION	UNIT	I-24	US 41A	RAMP A	RAMP B	RAMP C & C1	RAMP D & D1	RAMP E	RAMP F						PROJECT TOTALS
78	CRUSHED AGGREGATE SIZE NO 2 ^①	TON						3								3
1000	PERFORATED PIPE - 4 IN	LF						967								967
1010	NON-PERFORATED PIPE - 4 IN	LF						74								74
1015	INSPECT & CERTIFY EDGE DRAIN SYSTEM	LS														1
1028	PERF PIPE HEADWALL TY 3 - 4 IN	EACH						3								3
1812	REMOVE CURB AND GUTTER	LF						272								272
1845	ISLAND INTEGRAL CURB	LF	50 ^②					272	25 ^②							347
1982	DELINEATOR FOR GUARDRAIL - WHITE	EACH			6	2	19	22	12	14						75
1983	DELINEATOR FOR GUARDRAIL - YELLOW	EACH	6		4	4	4	8	12	14						52
1984	DELINEATOR FOR BARRIER - WHITE	EACH						16								16
1985	DELINEATOR FOR BARRIER - YELLOW	EACH						16								16
2003	RELOCATE TEMP CONC BARRIER ^③	LF						2,640								2,640
2014	BARRICADE - TYPE III	EACH						1								1
2159	TEMP DITCH	LF						1,325								1,325
2165	REMOVE PAVED DITCH	SY						343								343
2200	ROADWAY EXCAVATION ^④	CY	77		17	19	32	2,471	20	17						2,653
2230	EMBANKMENT IN PLACE ^⑤	CY		1,260				619								1,879
2237	DITCHING ^⑦	LF	6582	8850	2478	3256	4557	5082	3888	3293						37,986
2242	WATER ^⑧	MGAL	0.9	2.4	0.7	0.8	1.3	1.5	0.9	1						9.5
21802EN	G/R STEEL W BEAM-S FACE (7 FT POST) ^⑨	LF	1,962.5	87.5	950	512.5	2,100	2,837.5	2,250	1,662.5						12,362.5
2351	GUARDRAIL - STEEL W BEAM - S FACE ^②	LF	125						25							150
2352	GUARDRAIL - STEEL W BEAM - D FACE	LF	275	275												550
2360	GUARDRAIL TERMINAL SECTION NO.1	EACH	2													2
2363	GUARDRAIL CONNECTOR TO BRIDGE END TY A	EACH	3						1							4
2365	CRASH CUSHION TYPE IX-A	EACH	2	2												4
2369	GUARDRAIL END TREATMENT TYPE 2A	EACH	2		2		2	2	1	1						10
2381	REMOVE GUARDRAIL	LF	2,237.5	362.5	950	512.5	2,100	2,837.5	2,250	1,662.5						12,912.5
2387	GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	EACH	2													2
2391	GUARDRAIL END TREATMENT TYPE 4A	EACH	1	1	2	2	1	1	2	2						12
2395	REMOVE GUARDRAIL TERMINAL SECT	EACH	2													2
2396	REMOVE GUARDRAIL END TREATMENT	EACH	3	1	4	2	3	3	3	3						22
2483	CHANNEL LINING CLASS II ^⑩	TON						347.4								347.4
2562	SIGNS	SF														164
2568	MOBILIZATION	LS														1
2569	DEMOBILIZATION	LS														1
2650	MAINTAIN & CONTROL TRAFFIC	LS														1
2653	LANE CLOSURE	EACH	8													8
2671	PORTABLE CHANGEABLE MESSAGE SIGN ^⑬	EACH	2	2												4
2701	TEMP SILT FENCE	LF	2,194	2,950	1,703	1,878	3,190	2,200	2,338	2,543						18,996
2703	SILT TRAP TYPE A	EACH						3								3
2704	SILT TRAP TYPE B	EACH						3								3
2705	SILT TRAP TYPE C	EACH	4	5		2	2	1								14
2706	CLEAN SILT TRAP TYPE A	EACH						3								3
2707	CLEAN SILT TRAP TYPE B	EACH						3								3
2708	CLEAN SILT TRAP TYPE C	EACH	4	5		2	2	1								14
2709	CLEAN TEMP SILT FENCE	LF	2,194	2,950	1,703	1,878	3,190	2,200	2,338	2,543						18,996

NOTES:

- ① ESTIMATED AT 1 TON PER PERFORATED PIPE HEADWALL
- ② FOR GUARDRAIL CONNECTOR TO BRIDGE END TYPE A-1 AND BRIDGE END TYPE A. SEE STANDARD DRAWING RBC-001-09
- ③ SEE MOT SHEET NOS. R22-R31.
- ④ FOR I-24 AND ALL RAMP EXCEPT D & D1, QUANTITY ACCOUNTS FOR AREAS WHERE SUBGRADE IS UNSUITABLE AND MUST BE REPLACED. SEE NOTE 6 ON THE PAVING SUMMARY.
FOR RAMP D & D1, NOTE 8 ON THE PAVING SUMMARY APPLIES. ALSO INCLUDES EXIST. SHOULDER REMOVAL RAMP D 13+75 TO 26+80 LT, RAMP D 21+75 TO 24+00 RT AND RAMP D1 0+00 TO 4+00 RT. 233 SY OF QUANTITY FOR RAMP D & D1 IS FOR EXCAVATION OF UNSUITABLE SUBGRADE FOR ALL OF RAMP D & D1.
- ⑤ QUANTITY INCLUDES 1,260 CY FOR MEDIAN CROSS-OVER CONSTRUCTION ON US 41A AT RAMP D AND 619 CY FOR TEMPORARY WIDENING FOR RAMP D MAINTENANCE OF TRAFFIC.
- ⑥ NOT USED.
- ⑦ QUANTITY ESTIMATED FOR DITCHES NOT REQUIRING CHANNEL LINING
- ⑧ FOR CONTROLLING DUST CAUSED BY MAINTAINING TRAFFIC ONLY. ESTIMATED AT 2.125 MGAL/MILE.
- ⑨ CONTRARY TO STANDARD DRAWINGS, GUARDRAIL SHALL HAVE 7' POSTS.
- ⑩ ESTIMATED FOR PROPOSED DITCHES ON RAMP D PLUS AN ADDITIONAL 20% ALLOWING FOR REPAIRS OF ERODED DITCHES.
- ⑪ NOT USED
- ⑫ NOT USED
- ⑬ ALL PORTABLE CHANGEABLE MESSAGE SIGNS WILL BECOME THE PROPERTY OF THE CONTRACTOR AT PROJECTION COMPLETION.

SEE PAVING SUMMARY FOR PAVEMENT QUANTITIES

GENERAL SUMMARY

FILE NAME: G:\PWORK\VALEXI.SMITH\001445\TYTTC.SHEET.CEL

USER: alexi.smith
DATE PLOTTED: April 1, 2010

E-SHEET NAME:

MicroStation v8.11.7.180

GENERAL SUMMARY

COUNTY OF	ITEM NO.	SHEET NO.
CHRISTIAN	2-213.00	R2d

△ REVISED 5-3-11

ITEM	DESCRIPTION	UNIT	I-24	US 41A	RAMP A	RAMP B	RAMP C & C1	RAMP D & D1	RAMP E	RAMP F						PROJECT TOTALS
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1010	NON-PERFORATED PIPE - 4 IN	LF						74 △								74 △
1015	INSPECT & CERTIFY EDGE DRAIN SYSTEM	LS														1
1028	PERF PIPE HEADWALL TY 3 - 4 IN	EACH						3								3
1812	REMOVE CURB AND GUTTER	LF						272								272
1845	ISLAND INTEGRAL CURB	LF	50 ②					272	25 ②							347
1982	DELINEATOR FOR GUARDRAIL - WHITE	EACH			6	2	19	22	12	14						75
1983	DELINEATOR FOR GUARDRAIL - YELLOW	EACH	6		4	4	4	8	12	14						52
1984	DELINEATOR FOR BARRIER - WHITE	EACH						16								16
1985	DELINEATOR FOR BARRIER - YELLOW	EACH						16								16
2003	RELOCATE TEMP CONC BARRIER ③	LF						2,640								2,640
2014	BARRICADE - TYPE III	EACH						1								1
2159	TEMP DITCH	LF						1,325								1,325
2165	REMOVE PAVED DITCH	SY						343								343
2200	ROADWAY EXCAVATION ④	CY	77		17	19	32	2,471	20	17						2,653
2230	EMBANKMENT IN PLACE ⑤	CY		1,260				619								1,879
2237	DITCHING ⑦	LF	6582	8850	2478	3256	4557	5082	3888	3293						37,986
2242	WATER ⑧	MGAL	0.9	2.4	0.7	0.8	1.3	1.5	0.9	1						9.5
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2352	GUARDRAIL - STEEL W BEAM - D FACE	LF	275	275												550
2360	GUARDRAIL TERMINAL SECTION NO.1	EACH	2													2
2363	GUARDRAIL CONNECTOR TO BRIDGE END TY A	EACH	3						1							4
2365	CRASH CUSHION TYPE IX-A	EACH	2	2												4
2369	GUARDRAIL END TREATMENT TYPE 2A	EACH	2		2		2	2	1	1						10
2381	REMOVE GUARDRAIL	LF	2,237.5	362.5	950	512.5	2,100	2,837.5	2,250	1,662.5						12,912.5
2387	GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	EACH	2													2
2391	GUARDRAIL END TREATMENT TYPE 4A	EACH	1	1	2	2	1	1	2	2						12
2395	REMOVE GUARDRAIL TERMINAL SECT	EACH	2													2
2396	REMOVE GUARDRAIL END TREATMENT	EACH	3	1	4	2	3	3	3	3						22
2483	CHANNEL LINING CLASS II ⑩	TON						347.4								347.4
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2568	MOBILIZATION	LS														1
2569	DEMobilIZATION	LS														1
2650	MAINTAIN & CONTROL TRAFFIC	LS														1
2653	LANE CLOSURE	EACH	8													8
2671	PORTABLE CHANGEABLE MESSAGE SIGN ⑬	EACH	2	2												4
2701	TEMP SILT FENCE	LF	2,194	2,950	1,703	1,878	3,190	2,200	2,338	2,543						18,996
2703	SILT TRAP TYPE A	EACH						3								3
2704	SILT TRAP TYPE B	EACH						3								3
2705	SILT TRAP TYPE C	EACH	4	5		2	2	1								14
2706	CLEAN SILT TRAP TYPE A	EACH						3								3
2707	CLEAN SILT TRAP TYPE B	EACH						3								3
2708	CLEAN SILT TRAP TYPE C	EACH	4	5		2	2	1								14
2709	CLEAN TEMP SILT FENCE	LF	2,194	2,950	1,703	1,878	3,190	2,200	2,338	2,543						18,996

NOTES:

- ① ESTIMATED AT 1 TON PER PERFORATED PIPE HEADWALL
- ② FOR GUARDRAIL CONNECTOR TO BRIDGE END TYPE A-1 AND BRIDGE END TYPE A. SEE STANDARD DRAWING RBC-001-09
- ③ SEE MOT SHEET NOS. R22-R31.
- ④ FOR I-24 AND ALL RAMP EXCEPT D & DI, QUANTITY ACCOUNTS FOR AREAS WHERE SUBGRADE IS UNSUITABLE AND MUST BE REPLACED. SEE NOTE 6 ON THE PAVING SUMMARY.

FOR RAMP D & DI, NOTE 8 ON THE PAVING SUMMARY APPLIES. ALSO INCLUDES EXIST. SHOULDER REMOVAL RAMP D 13+75 TO 26+80 LT, RAMP D 21+75 TO 24+00 RT AND RAMP DI 0+00 TO 4+00 RT. 233 SY OF QUANTITY FOR RAMP D & DI IS FOR EXCAVATION OF UNSUITABLE SUBGRADE FOR ALL OF RAMP D & DI.
- ⑤ QUANTITY INCLUDES 1,260 CY FOR MEDIAN CROSS-OVER CONSTRUCTION ON US 41A AT RAMP D AND 619 CY FOR TEMPORARY WIDENING FOR RAMP D MAINTENANCE OF TRAFFIC.
- ⑥ NOT USED.
- ⑦ QUANTITY ESTIMATED FOR DITCHES NOT REQUIRING CHANNEL LINING
- ⑧ FOR CONTROLLING DUST CAUSED BY MAINTAINING TRAFFIC ONLY. ESTIMATED AT 2.125 MGAL/MILE.
- ⑨ CONTRARY TO STANDARD DRAWINGS, GUARDRAIL SHALL HAVE 7' POSTS.
- ⑩ ESTIMATED FOR PROPOSED DITCHES ON RAMP D PLUS AN ADDITIONAL 20% ALLOWING FOR REPAIRS OF ERODED DITCHES.
- ⑪ NOT USED
- ⑫ NOT USED
- ⑬ ALL PORTABLE CHANGEABLE MESSAGE SIGNS WILL BECOME THE PROPERTY OF THE CONTRACTOR AT PROJECTION COMPLETION.

SEE PAVING SUMMARY FOR PAVEMENT QUANTITIES

GENERAL SUMMARY

FILE NAME: G:\PWORK\VALEXI.SMITH\0014451\KTYC.SHEET.CEL
 USER: alexl.smith
 DATE PLOTTED: April 1, 2010
 E-SHEET NAME:
 MicroStation v8.11.7.180

PAVING AREAS

ITEM	I-24	US 41A	CROSS-OVER AT RAMP C	CROSS-OVER AT RAMP D	RAMP A	RAMP B	RAMP C & C1	RAMP D & D1	RAMP E	RAMP F	TOTAL PROJECT
TRAFFIC LANES											
JPC PAVEMENT - 10 INCH ⑤ ⑥	1,145			235	255	288	479	4,007	295	260	6,964
JPC PAVEMENT - 6 INCH ⑨		3,253		163							3,416
1.5" CL4 ASPH SURF 0.5A PG64-22		12,480	177								12,657
DGA	229			268	51	58	96	1,728	59	52	2,541
LEVELING & WEDGING PG 64-22 ⑭ ①		12,480	177								12,657
SHOULDERS											
JPC PAVEMENT - 10 INCH ⑧				27.4				1,282			1,309.4
ASPHALT SEAL AGGREGATE	870	5,244			511	270	947	2,430	1,057	773	12,102
EMULSIFIED ASPHALT RS-2	870	5,244			511	270	947	2,430	1,057	773	12,102
DGA				207.7				3,779			3,986.7
1.5" CL4 ASPHALT SURFACE 0.5A PG 64-22		7,867	20	6.9							7,893.9
LEVELING & WEDGING PG 64-22 ⑭ ①		7,867	20	6.9							7,893.9
TEMPORARY PAVEMENT - RAMP D											
12" DGA BASE								2,356			2,356
4" CL3 ASPH BASE 1.00D PG 64-22								1,521			1,521

PAVING SUMMARY

ITEM CODE	ITEM	UNIT	I-24	US 41A	CROSS-OVER AT RAMP C	CROSS-OVER AT RAMP D	RAMP A	RAMP B	RAMP C & C1	RAMP D & D1	RAMP E	RAMP F	TOTAL PROJECT
1	DGA	TON	158	185		334.5	35	40	66	2,899	41	36	3,794.5
1	DGA BASE FOR TEMPORARY WIDENING	TON								1,538			1,538
100	ASPHALT SEAL AGGREGATE ③	TON	17	105			10	5	19	49	21	16	242
190	LEVELING & WEDGING PG 64-22 ⑭ ①	TON		279	2.4								281.4
214	4" CL3 ASPH BASE 1.00D PG64-22	TON								335			335
291	EMULSIFIED ASPHALT RS-2 ④	TON	2	13		1	1	1	2	6	3	2	30
333	1.5" CL4 ASPH SURF 0.5A PG64-22 ①	TON		1,680	17.2								1,697.2
2058	REMOVE PCC PAVEMENT ⑤	SY	1,145				255	288	479	4,132	295	260	6,584
2060	PCC PAVEMENT DIAMOND GRINDING	SY	11,454				2,553	2,877	4,787	3,231	2,946	2,598	30,446
2069	JPC PAVEMENT - 10 INCH ⑤ ⑧	SY	1,145		235	255	288	479	5,289	295	260	8,246	
2075	JPC PAVEMENT - 6 INCH ⑨	SY		3,253	163								3,416
2091	REMOVE PAVEMENT ⑮	SY								2,356			2,356
2115	SAW-CLEAN-RESEAL TVERSE JOINT ⑩	LF	6,341				2,339	2,396	3,392	2,122	2,411	2,192	21,193
2116	SAW-CLEAN-RESEAL LONGIT JOINT ⑩	LF	3,964				3,257	2,550	4,271	1,960	2,779	2,961	21,742
2599	FABRIC - GEOTEXTILE TYPE IV ⑪	SY	229			217.8	51	58	96	1,855	59	52	2,617.8
2676	MOBILIZATION FOR MILL & TEXT	LS		1									1
2677	ASPHALT PAVE MILLING & TEXTURING ①	TON		2,754	17.2	54.6							2,825.8
8100	CONCRETE-CLASS A ⑦	CY								58			58

- ① ESTIMATED AT 110 LBS/SY/IN.
- ② ESTIMATED AT 115 LBS/SY/IN.
- ③ ESTIMATED AT 20 LBS/SY
- ④ ESTIMATED AT 2.4 LBS/SY
- ⑤ AREAS REQUIRING JPC REPLACEMENT ARE ESTIMATED TO BE 10% OF THE TOTAL AREA TO BE DIAMOND GROUND.
- ⑥ QUANTITY INCLUDES AN ESTIMATION THAT 20% OF THE AREA REQUIRING JPC REPLACEMENT WILL REQUIRE 12" SUBGRADE REPLACEMENT. SEE TYPICAL SECTIONS.
- ⑦ 4" CONCRETE CAP OVER EXISTING ISLAND
- ⑧ RAMP D & D1 QUANTITIES:
- RAMP D STA. 0+00 TO 16+00 - QUANTITIES ASSUME NOTES 5 AND 6.
- RAMP D STA. 16+00 TO 21+75 - TOTAL DRIVING LANE AND SHOULDER TO HAVE JPC REPLACEMENT. DGA QUANTITY ASSUMES 20% OF THE AREA WILL REQUIRE 12" SUBGRADE REPLACEMENT.
- RAMP D STA. 21+75 TO 26+85 AND RAMP D1 STA. 0+00 TO 4+00 - FULL DEPTH QUANTITIES FOR WIDENED SECTION. TOTAL EXISTING DRIVING LANE AND SHOULDER AREAS TO HAVE JPC REPLACEMENT. DGA QUANTITY ASSUMES 20% OF THE JPC REPLACEMENT AREA WILL REQUIRE 12" SUBGRADE REPLACEMENT.
- RAMP D1 STA. 4+00 TO 9+37 - QUANTITIES ASSUME NOTES 5 AND 6.
- ⑨ QUANTITY FOR CONCRETE INLAY ON US 41A. SEE SHEET NO. R5 FOR LIMITS. ALSO INCLUDES QUANTITY FOR EXISTING PORTION OF MEDIAN CROSS-OVER AT INTERSECTION WITH RAMP D.
- ⑩ INCLUDES ALL AREAS FOR DIAMOND GRINDING WHERE CONCRETE IS NOT REPLACED.
- ⑪ INCLUDES AREAS WHERE SUBGRADE IS REPLACED AND FOR WIDENING OF RAMP D.
- ⑫ NOT USED
- ⑬ NOT USED
- ⑭ ASSUMES AN AVERAGE 1/4" DEPTH FOR TOTAL AREA OF US 41A THAT IS ASPHALT.
- ⑮ FOR REMOVAL OF TEMPORARY PAVEMENT.

PAVING AREAS

ITEM	I-24	US 41A	CROSS-OVER AT RAMP C	CROSS-OVER AT RAMP D	RAMP A	RAMP B	RAMP C & C1	RAMP D & D1	RAMP E	RAMP F	TOTAL PROJECT
TRAFFIC LANES											
JPC PAVEMENT - 10 INCH ⑤ ⑥	1,145			235	255	288	479	4,007	295	260	6,964
JPC PAVEMENT - 6 INCH ⑨		3,253		163							3,416
1.5" CL4 ASPH SURF 0.5A PG64-22		12,480	177								12,657
DGA	229			268	51	58	96	1,728	59	52	2,541
LEVELING & WEDGING PG 64-22 ⑭ ①	12,480	177									12,657
SHOULDERS											
JPC PAVEMENT - 10 INCH ⑥				27.4				1,282			1,309.4
ASPHALT SEAL AGGREGATE	870	5,244			511	270	947	2,430	1,057	773	12,102
EMULSIFIED ASPHALT RS-2	870	5,244		207.7	511	270	947	2,430	1,057	773	12,102
DGA	7,867	20		6.9				3,779			3,986.7
1.5" CL4 ASPHALT SURFACE 0.5A PG 64-22											7,893.9
LEVELING & WEDGING PG 64-22 ⑭ ①		7,867	20	6.9							7,893.9
TEMPORARY PAVEMENT - RAMP D											
12" DGA BASE								2,356			2,356
4" CL3 ASPH BASE 1.00D PG 64-22								1,521			1,521

PAVING SUMMARY

ITEM CODE	ITEM	UNIT	I-24	US 41A	CROSS-OVER AT RAMP C	CROSS-OVER AT RAMP D	RAMP A	RAMP B	RAMP C & C1	RAMP D & D1	RAMP E	RAMP F	TOTAL PROJECT
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1	DGA BASE FOR TEMPORARY WIDENING	TON								1,538			1,538
100	ASPHALT SEAL AGGREGATE ③	TON	17	105			10	5	19	49	21	16	242
190	LEVELING & WEDGING PG 64-22 ⑭ ①	TON		279	2.4								281.4
214	4" CL3 ASPH BASE 1.00D PG64-22	TON								335			335
291	EMULSIFIED ASPHALT RS-2 ④	TON	2	13			1	1	2	6	3	2	30
333	1.5" CL4 ASPH SURF 0.5A PG64-22 ①	TON		1,680	17.2			255	288	4,132	295	260	1,697.2
2058	REMOVE PCC PAVEMENT ⑤	SY	1,145				2,553	2,877	4,787	3,231	2,946	2,598	6,584
2060	PCC PAVEMENT DIAMOND GRINDING	SY	1,145				235	255	288	479	5,289	295	30,446
2069	JPC PAVEMENT - 10 INCH ⑤ ⑧	SY				163							8,246
2075	JPC PAVEMENT - 6 INCH ⑨	SY		3,253									3,416
2091	REMOVE PAVEMENT ⑮	SY								2,356			2,356
2115	SAW-CLEAN-RESEAL TVERSE JOINT ⑩	LF	6,341				2,339	2,396	3,392	2,122	2,411	2,192	21,193
2116	SAW-CLEAN-RESEAL LONGIT JOINT ⑩	LF	3,964				3,257	2,550	4,271	1,960	2,779	2,961	21,742
2599	FABRIC - GEOTEXTILE TYPE IV ⑪	SY	229				51	58	96	1,855	59	52	2,617.8
2676	MOBILIZATION FOR MILL & TEXT	LS		1									1
2677	ASPHALT PAVE MILLING & TEXTURING ①	TON		2,754	17.2	54.6							2,825.8
8100	CONCRETE-CLASS A ⑦	CY								58			58

- ① ESTIMATED AT 110 LBS/SY/IN.
- ② ESTIMATED AT 115 LBS/SY/IN.
- ③ ESTIMATED AT 20 LBS/SY
- ④ ESTIMATED AT 2.4 LBS/SY
- ⑤ AREAS REQUIRING JPC REPLACEMENT ARE ESTIMATED TO BE 10% OF THE TOTAL AREA TO BE DIAMOND GROUND.
- ⑥ QUANTITY INCLUDES AN ESTIMATION THAT 20% OF THE AREA REQUIRING JPC REPLACEMENT WILL REQUIRE 12" SUBGRADE REPLACEMENT. SEE TYPICAL SECTIONS.
- ⑦ 4" CONCRETE CAP OVER EXISTING ISLAND

- ⑧ RAMP D & D1 QUANTITIES:
- RAMP D STA. 0+00 TO 16+00 - QUANTITIES ASSUME NOTES 5 AND 6.
- RAMP D STA. 16+00 TO 21+75 - TOTAL DRIVING LANE AND SHOULDER TO HAVE JPC REPLACEMENT. DGA QUANTITY ASSUMES 20% OF THE AREA WILL REQUIRE 12" SUBGRADE REPLACEMENT.
- RAMP D STA. 21+75 TO 26+85 AND RAMP D1 STA. 0+00 TO 4+00 - FULL DEPTH QUANTITIES FOR WIDENED SECTION. TOTAL EXISTING DRIVING LANE AND SHOULDER AREAS TO HAVE JPC REPLACEMENT. DGA QUANTITY ASSUMES 20% OF THE JPC REPLACEMENT AREA WILL REQUIRE 12" SUBGRADE REPLACEMENT.
- RAMP D1 STA. 4+00 TO 9+37 - QUANTITIES ASSUME NOTES 5 AND 6.
- ⑨ QUANTITY FOR CONCRETE INLAY ON US 41A. SEE SHEET NO. R5 FOR LIMITS. ALSO INCLUDES QUANTITY FOR EXISTING PORTION OF MEDIAN CROSS-OVER AT INTERSECTION WITH RAMP D.
- ⑩ INCLUDES ALL AREAS FOR DIAMOND GRINDING WHERE CONCRETE IS NOT REPLACED.
- ⑪ INCLUDES AREAS WHERE SUBGRADE IS REPLACED AND FOR WIDENING OF RAMP D.
- ⑫ NOT USED
- ⑬ NOT USED
- ⑭ ASSUMES AN AVERAGE 1/4" DEPTH FOR TOTAL AREA OF US 41A THAT IS ASPHALT.
- ⑮ FOR REMOVAL OF TEMPORARY PAVEMENT.

GENERAL NOTES

COUNTY OF	ITEM NO.	SHEET NO.
CHRISTIAN	2-213.00	R2h

THIS PROJECT IS A FULLY CONTROLLED ACCESS HIGHWAY.

CONTROL OF WORK

THE RIGHT IS RESERVED BY THE DEPARTMENT TO HAVE OTHER WORK PERFORMED BY OTHER CONTRACTORS AND BY ITS OWN FORCES AND TO PERMIT PUBLIC UTILITY COMPANIES AND OTHERS TO DO WORK DURING THE CONSTRUCTION OF, AND WITHIN THE LIMITS OF OR ADJACENT TO, THE PROJECT. THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS AND COOPERATE WITH SUCH OTHER PARTIES SO THAT INTERFERENCE WITH SUCH OTHER WORK WILL BE REDUCED TO A MINIMUM. THE CONTRACTOR SHALL AGREE, AND HEREBY DOES AGREE, TO MAKE NO CLAIMS AGAINST THE DEPARTMENT FOR ADDITIONAL COMPENSATION DUE TO DELAYS OR OTHER CONDITIONS CREATED BY THE OPERATIONS OF SUCH PARTIES. SHOULD A DIFFERENCE OF OPINION ARISE AS TO THE RIGHTS OF THE CONTRACTOR AND OTHERS WORKING WITHIN THE LIMITS OF OR ADJACENT TO THE PROJECT, THE ENGINEER WILL DECIDE AS TO THE RESPECTIVE RIGHTS OF THE VARIOUS PARTIES INVOLVED IN ORDER TO ASSURE THE COMPLETION OF THE DEPARTMENT'S WORK IN GENERAL HARMONY AND IN A SATISFACTORY MANNER AND HIS DECISION SHALL BE FINAL AND BINDING UPON THE CONTRACTOR.

160 N.G.S. (U.S.G.S.) BENCH MARKS

DO NOT DISTURB N.G.S (U.S.G.S.) BENCH MARKS IN ANY MANNER UNLESS DIRECTED BY THE ENGINEER.

165 BEFORE YOU DIG

THE CONTRACTOR IS INSTRUCTED TO CALL 1-800-752-6007 TO REACH KY 811, THE ONE-CALL SYSTEM FOR INFORMATION ON THE LOCATION OF EXISTING UNDERGROUND UTILITIES. THE CALL IS TO BE PLACED A MINIMUM OF TWO (2) AND NO MORE THAN TEN (10) BUSINESS DAYS PRIOR TO EXCAVATION. THE CONTRACTOR SHOULD BE AWARE THAT OWNERS OF UNDERGROUND FACILITIES ARE NOT REQUIRED TO BE MEMBERS OF THE KY 811 ONE-CALL BEFORE-U-DIG (BUD) SERVICE. THE CONTRACTOR MUST COORDINATE EXCAVATION WITH THE UTILITY OWNERS, INCLUDING THOSE WHOM DO NOT SUBSCRIBE TO KY 811. IT MAY BE NECESSARY FOR THE CONTRACTOR TO CONTACT THE COUNTY COURT CLERK TO DETERMINE WHAT UTILITY COMPANIES HAVE FACILITIES IN THE AREA.

190 DEPARTMENT OF THE ARMY PERMIT AND WATER QUALITY CERTIFICATION APPROVALS

A DEPARTMENT OF THE ARMY (DA) PERMIT, WHICH MAY REQUIRE APPROVAL OF A STATE WATER QUALITY CERTIFICATION FROM THE KENTUCKY DIVISION OF WATER, REGULATES THIS PROJECT AT ONE OR MORE LOCATIONS. PERFORM ALL APPLICABLE WORK IN COMPLIANCE WITH THE CONDITIONS STATED IN THE DA PERMIT AND THE APPROVED WATER QUALITY CERTIFICATION. POST A COPY OF THE DA PERMIT AND THE WATER QUALITY CERTIFICATION IN A CONSPICUOUS PLACE AT THE PROJECT SITE. IF A DA PERMIT OR WATER QUALITY CERTIFICATION APPROVAL IS PENDING, DO NOT WORK IN OR DISTURB THE DESIGNATED AREA(S) UNTIL OBTAINING THE APPROPRIATE APPROVAL(S). REFER TO NOTICE(S) CONTAINED IN THE CONTRACT BID PROPOSAL FOR DESIGNATED AREA(S) WHERE WORK IS PROHIBITED BY THE ABSENCE OF APPROVAL.

200 CLEARING AND GRUBBING

CONTRARY TO SECTION 202 OF THE STANDARD SPECIFICATIONS, NO DIRECT PAYMENT WILL BE ALLOWED FOR CLEARING AND GRUBBING ON THIS PROJECT.

444 ASPHALT PAVEMENT RIDE QUALITY

PAVEMENT RIDEABILITY REQUIREMENTS, IN ACCORDANCE WITH SECTION 410 OF THE STANDARD SPECIFICATIONS, SHALL APPLY ON THIS PROJECT. CATEGORY A SHALL APPLY.

447 COMPACTION OF ASPHALT MIXTURES

WILL ACCEPT THE COMPACTION OF ASPHALT MIXTURES FURNISHED FOR DRIVING LANES AND RAMPS AT ONE INCH (25 MM) OR GREATER ON THIS PROJECT BY OPTION A ACCORDING TO SUBSECTIONS 402 AND 403 OF THE CURRENT STANDARD SPECIFICATIONS. USE JOINT CORES AS DESCRIBED IN SUBSECTION 402.03.02 FOR SURFACE MIXTURES ONLY. WILL ACCEPT THE COMPACTION OF ALL OTHER ASPHALT MIXTURES BY OPTION B.

555 JPC PAVEMENT QUALITY

APPLY JPC PAVEMENT SMOOTHNESS REQUIREMENTS, IN ACCORDANCE WITH SUBSECTION 501.03.19 OF THE STANDARD SPECIFICATIONS ON THIS PROJECT.

650 STANDARD DRAWINGS

STANDARD DRAWINGS ARE NOT ATTACHED TO THESE PLANS. A STANDARD DRAWING BOOK AND THE HEADWALL SUPPLEMENTAL BOOK MAY BE OBTAINED FROM THE POLICY SUPPORT BRANCH OF THE DEPARTMENT OF ADMINISTRATIVE SERVICES IN FRANKFORT, KY, AT (502) 564-3670

SPECIAL NOTES AND PROVISIONS

SPECIAL NOTE 11: PORTABLE CHANGEABLE MESSAGE SIGNS (1-1-2008)
 SPECIAL NOTE 9Y: MATERIAL TRANSFER VEHICLES (3-12-2008)
 SPECIAL NOTE 10T: ACCEPTANCE OF JPC PAVEMENT THICKNESS 2004 (1-1-2008)
 SPECIAL NOTE 10W: WATER BLASTING STRIPING REMOVAL (1-1-2008)
 SPECIAL NOTE: FULL DEPTH CONCRETE PAVEMENT REPAIR (8-5-2010)

BASIS OF PLANS

THE DIMENSIONS SHOWN ON THE TYPICAL SECTIONS FOR PAVEMENT AND SHOULDER WIDTHS AND THICKNESSES ARE NOMINAL OR TYPICAL DIMENSIONS. THE ACTUAL DIMENSIONS TO BE CONSTRUCTED MAY BE VARIED TO FIT EXISTING CONDITIONS AS DIRECTED OR APPROVED BY THE ENGINEER. IT IS NOT INTENDED THAT EXISTING PAVEMENT OR SHOULDERS BE WIDENED, EXCEPT AS SPECIFIED FOR RAMPS D AND D1.

GUARDRAIL REMOVAL

REMOVED EXIST. GUARDRAIL SHALL BE TRANSPORTED TO THE CENTRAL SIGN SHOP AND RECYCLE CENTER AT 1224 WILKINSON BOULEVARD IN FRANKFORT, KY. HAULING OF THESE ITEMS IS INCIDENTAL TO GUARDRAIL REMOVAL.

PAVEMENT MARKERS

ALL EXISTING PAVEMENT MARKERS ARE TO BE REMOVED AND REPLACED.

DELINEATORS

DELINEATORS SHALL MEET THE REQUIREMENTS OF SECTION 830 AND 838 OF THE STANDARD SPECIFICATIONS. DELINEATORS SHALL BE PLACED IN ACCORDANCE WITH SECTION 3D OF THE M.U.T.C.D.

LEVELING AND WEDGING

THE QUANTITY OF LEVELING AND WEDGING MATERIAL IS INTENDED TO CORRECT IRREGULARITIES IN THE EXISTING ROADWAY SURFACE TO AID IN RIDEABILITY. IT IS NOT INTENDED TO BRING THE ENTIRE ROADWAY SURFACE TO THE AS-BUILT ELEVATIONS. THIS MATERIAL IS NOT INTENDED TO BE USED AS A SCRATCH COURSE. THIS QUANTITY FOR IRREGULARITIES IS ESTIMATED AND MAY OR MAY NOT BE NECESSARY. THE RESIDENT ENGINEER WILL DETERMINE HOW MUCH OF THE MATERIAL IS NECESSARY.

PERFORATED PIPE DITCH

BACKFILL OF THE PERFORATED PIPE DITCH IS TO BE INCIDENTAL TO "PERFORATED PIPE - 4 INCH."

CLEANING OF CULVERT INLETS

THE CLEANING OF INLETS AND OUTLETS OF EXISTING PIPE CULVERTS, THE CBI TYPE A ON THE RAMP D ISLAND AND THE EXISTING INLET TO BE CAPPED ON US 41A IS INCIDENTAL TO THE BID ITEM FOR "DITCHING".

CHANGEABLE MESSAGE BOARDS

CHANGEABLE MESSAGE BOARDS FURNISHED BY THE CONTRACTOR SHALL BE RETAINED BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT.

NEW GUARDRAIL PLACEMENT

GUARDRAIL, END TREATMENTS AND TERMINAL SECTIONS TO BE REPLACED ARE LISTED BY STATIONS. EXACT PLACEMENT TO BE APPROVED BY THE ENGINEER ON CONSTRUCTION. GUARDRAIL POSTS ARE NOT TO IMPACT EXISTING LIGHTING CONDUITS OR PAVEMENT EDGE DRAINS.

SHORT-TERM LANE CLOSURES

SHORT-TERM LANE CLOSURES WILL BE ALLOWED ON US 41A. COSTS INCIDENTAL TO MAINTENANCE OF TRAFFIC.

LANE CLOSURES

THE NUMBER OF LANE CLOSURES SET UP FOR THE I-24 CONSTRUCTION ARE BASED UPON A CONCEPTUAL PROGRESSION OF THE PROJECT. THE CONTRACTOR WILL NOT BE PAID AN ADDITIONAL NUMBER OF LANE CLOSURES SHOULD ANY ALTERATION OF THE PROJECT PHASING BE DESIRABLE FOR THE CONTRACTOR'S PROGRESSION OF WORK UNLESS REVIEWED AND APPROVED BY THE ENGINEER.

EXISTING SIGNS

THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THE EXISTING SHEET SIGNS THROUGHOUT THE LIMITS OF THIS PROJECT. PAYMENT FOR THIS IS INCIDENTAL TO THE BID ITEM "MAINTAIN AND CONTROL TRAFFIC".

EXISTING PAVEMENT REMOVAL

THE BID ITEM "REMOVE PCC PAVEMENT" WILL BE APPLICABLE FOR THE REMOVAL OF MAINLINE AND RAMP PAVEMENT EXCEPT US 41A. THE REMOVAL OF PAVEMENT ON US 41A WILL BE PAID FOR UNDER BID ITEM "ASPHALT MILLING AND TEXTURING". THE SUBGRADE EXCAVATION WILL BE PAID UNDER THE BID ITEM "ROADWAY EXCAVATION".

PROJECT ACCESS

THE CONTRACTOR WILL BE ALLOWED TO ACCESS THE PROJECT AND CROSS THE MEDIAN PROVIDED PROPER LANE CLOSURES AND FLAGMEN ARE UTILIZED AT THE ENTRY AND EXIT LOCATIONS.

PIPELINE VIDEO INSPECTION

THE CONTRACTOR SHALL PERFORM VIDEO INSPECTION ON NEW PIPE CONSTRUCTION ONLY.

TRAFFIC CONTROL ITEMS

UNLESS OTHERWISE DIRECTED, ALL SALVAGEABLE TRAFFIC CONTROL ITEMS, DEVICES, MATERIALS AND INCIDENTALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR WHEN NO LONGER NEEDED FOR MAINTAINING AND CONTROLLING TRAFFIC DURING CONSTRUCTION.

MOWING CONTRACT

THERE WILL BE A MOWING CONTRACT THROUGH THIS PROJECT. THE CONTRACTOR WILL ALLOW THE MOWING CONTRACTOR TO COMPLETE THE MOWING THROUGH THIS AREA.

COMPLETION DATE

THE SPECIFIED COMPLETION DATE FOR THIS PROJECT IS NOVEMBER 15, 2011. ALL ASPHALT SURFACING SHALL BE COMPLETED BY NOVEMBER 1, 2011. CONTRARY TO SECTION 108.09 OF THE STANDARD SPECIFICATIONS, LIQUIDATED DAMAGES WILL BE ASSESSED FROM DECEMBER THROUGH MARCH.

CONSTRUCTION SCHEDULE

WORK ON THE PROJECT MAY BE SUSPENDED BY THE ENGINEER DURING HOLIDAYS AND OTHER PERIODS OF TIME THAT MAY BE DESIRABLE. THESE INCLUDE:

MEMORIAL DAY WEEKEND MAY 28TH - 30TH, 2011
 INDEPENDENCE HOLIDAY WEEKEND JULY 2ND - 4TH, 2011
 LABOR DAY WEEKEND SEPTEMBER 3RD - 5TH, 2011
 THANKSGIVING HOLIDAY NOVEMBER 24TH - 27TH, 2011
 CHRISTMAS HOLIDAY DECEMBER 24TH - 26TH, 2011
 NEW YEAR'S HOLIDAY DECEMBER 31, 2011 - JANUARY 1ST, 2012

LIQUIDATED DAMAGES AND DISINCENTIVES

I-24 AND US 41A ARE TO REMAIN OPEN AT ALL TIMES THROUGHOUT CONSTRUCTION.

DAMAGES FOR CONSTRUCTION SHALL BE ASSESSED AT A RATE OF \$1,000 PER HOUR FOR EACH HOUR THE PHASE IS IN VIOLATION OF THE TIME FRAMES DESCRIBED. AFTER THE FIRST 24 HOURS THE CONTRACTOR WILL BE ASSESSED \$24,000 PER DAY.

JOINT SEALING

ALL JOINT SEALING SHALL BE PERFORMED USING HOT POURED ELASTIC JOINT SEALERS, IN ACCORDANCE WITH SECTION 807 OF THE SPECIFICATIONS, REGARDLESS OF THE WIDTH OF THE JOINT. ANY ADDITIONAL MATERIALS, LABOR, AND EQUIPMENT REQUIRED TO SEAL THE JOINTS DUE TO VARIATIONS IN JOINT WIDTH SHALL BE INCIDENTAL TO THE BID ITEMS: SAW, CLEAN & RESEAL TRANSVERSE JOINTS AND SAW, CLEAN & RESEAL LONGITUDINAL JOINTS.

DIAMOND GRINDING SLURRY

THE DIAMOND GRINDING SLURRY MAY BE WASTED ON THE SHOULDERS WHERE POSSIBLE.

MEDIAN CROSS-OVERS ON US 41A

ALL QUANTITIES RELATED TO MEDIAN CROSS-OVER CONSTRUCTION ARE INCORPORATED IN THE INDIVIDUAL BID ITEMS. THE QUANTITIES ARE DESIGNATED AS SUCH ON THE GENERAL SUMMARY BY NOTES. ON THE PAVING SUMMARY THE QUANTITIES ARE SEPARATE FROM THE US 41A QUANTITIES.

GENERAL NOTES

FILE NAME: G:\P\WORK\VALEXI.SMITH\00144517\KYTC-SHEET.CEL

USER: Alexi.Smith
 DATE PLOTTED: January 1, 2011

E-SHEET NAME:

MicroStation v8.1i.7.180

GENERAL NOTES

COUNTY OF	ITEM NO.	SHEET NO.
CHRISTIAN	2-213.00	R2h

▲ REVISED 5-3-11
DELETED SPECIAL NOTE 10Y
DELETED SPECIAL PROVISION 76
ADDED SPECIAL NOTE: FULL DEPTH CONCRETE PAVEMENT REPAIR

THIS PROJECT IS A FULLY CONTROLLED ACCESS HIGHWAY.

CONTROL OF WORK

THE RIGHT IS RESERVED BY THE DEPARTMENT TO HAVE OTHER WORK PERFORMED BY OTHER CONTRACTORS AND BY ITS OWN FORCES AND TO PERMIT PUBLIC UTILITY COMPANIES AND OTHERS TO DO WORK DURING THE CONSTRUCTION OF, AND WITHIN THE LIMITS OF OR ADJACENT TO, THE PROJECT. THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS AND COOPERATE WITH SUCH OTHER PARTIES SO THAT INTERFERENCE WITH SUCH OTHER WORK WILL BE REDUCED TO A MINIMUM. THE CONTRACTOR SHALL AGREE, AND HEREBY DOES AGREE, TO MAKE NO CLAIMS AGAINST THE DEPARTMENT FOR ADDITIONAL COMPENSATION DUE TO DELAYS OR OTHER CONDITIONS CREATED BY THE OPERATIONS OF SUCH PARTIES. SHOULD A DIFFERENCE OF OPINION ARISE AS TO THE RIGHTS OF THE CONTRACTOR AND OTHERS WORKING WITHIN THE LIMITS OF OR ADJACENT TO THE PROJECT, THE ENGINEER WILL DECIDE AS TO THE RESPECTIVE RIGHTS OF THE VARIOUS PARTIES INVOLVED IN ORDER TO ASSURE THE COMPLETION OF THE DEPARTMENT'S WORK IN GENERAL HARMONY AND IN A SATISFACTORY MANNER AND HIS DECISION SHALL BE FINAL AND BINDING UPON THE CONTRACTOR.

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THE CONTRACTOR IS INSTRUCTED TO CALL 1-800-752-6007 TO REACH KY 811, THE ONE-CALL SYSTEM FOR INFORMATION ON THE LOCATION OF EXISTING UNDERGROUND UTILITIES. THE CALL IS TO BE PLACED A MINIMUM OF TWO (2) AND NO MORE THAN TEN (10) BUSINESS DAYS PRIOR TO EXCAVATION. THE CONTRACTOR SHOULD BE AWARE THAT OWNERS OF UNDERGROUND FACILITIES ARE NOT REQUIRED TO BE MEMBERS OF THE KY 811 ONE-CALL BEFORE-U-DIG (BUD) SERVICE. THE CONTRACTOR MUST COORDINATE EXCAVATION WITH THE UTILITY OWNERS, INCLUDING THOSE WHOM DO NOT SUBSCRIBE TO KY 811. IT MAY BE NECESSARY FOR THE CONTRACTOR TO CONTACT THE COUNTY COURT CLERK TO DETERMINE WHAT UTILITY COMPANIES HAVE FACILITIES IN THE AREA.

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A DEPARTMENT OF THE ARMY (DA) PERMIT, WHICH MAY REQUIRE APPROVAL OF A STATE WATER QUALITY CERTIFICATION FROM THE KENTUCKY DIVISION OF WATER, REGULATES THIS PROJECT AT ONE OR MORE LOCATIONS. PERFORM ALL APPLICABLE WORK IN COMPLIANCE WITH THE CONDITIONS STATED IN THE DA PERMIT AND THE APPROVED WATER QUALITY CERTIFICATION. POST A COPY OF THE DA PERMIT AND THE WATER QUALITY CERTIFICATION IN A CONSPICUOUS PLACE AT THE PROJECT SITE. IF A DA PERMIT OR WATER QUALITY CERTIFICATION APPROVAL IS PENDING, DO NOT WORK IN OR DISTURB THE DESIGNATED AREA(S) UNTIL OBTAINING THE APPROPRIATE APPROVAL(S). REFER TO NOTICE(S) CONTAINED IN THE CONTRACT BID PROPOSAL FOR DESIGNATED AREA(S) WHERE WORK IS PROHIBITED BY THE ABSENCE OF APPROVAL.

200 CLEARING AND GRUBBING

CONTRARY TO SECTION 202 OF THE STANDARD SPECIFICATIONS, NO DIRECT PAYMENT WILL BE ALLOWED FOR CLEARING AND GRUBBING ON THIS PROJECT.

444 ASPHALT PAVEMENT RIDE QUALITY

PAVEMENT RIDEABILITY REQUIREMENTS, IN ACCORDANCE WITH SECTION 410 OF THE STANDARD SPECIFICATIONS, SHALL APPLY ON THIS PROJECT. CATEGORY A SHALL APPLY.

447 COMPACTION OF ASPHALT MIXTURES

WILL ACCEPT THE COMPACTION OF ASPHALT MIXTURES FURNISHED FOR DRIVING LANES AND RAMPS AT ONE INCH (25 MM) OR GREATER ON THIS PROJECT BY OPTION A ACCORDING TO SUBSECTIONS 402 AND 403 OF THE CURRENT STANDARD SPECIFICATIONS. USE JOINT CORES AS DESCRIBED IN SUBSECTION 402.03.02 FOR SURFACE MIXTURES ONLY. WILL ACCEPT THE COMPACTION OF ALL OTHER ASPHALT MIXTURES BY OPTION B.

555 JPC PAVEMENT QUALITY

APPLY JPC PAVEMENT SMOOTHNESS REQUIREMENTS, IN ACCORDANCE WITH SUBSECTION 501.03.19 OF THE STANDARD SPECIFICATIONS ON THIS PROJECT.

650 STANDARD DRAWINGS

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SPECIAL NOTES AND PROVISIONS

SPECIAL NOTE 11: PORTABLE CHANGEABLE MESSAGE SIGNS (1-1-2008)
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BASIS OF PLANS

THE DIMENSIONS SHOWN ON THE TYPICAL SECTIONS FOR PAVEMENT AND SHOULDER WIDTHS AND THICKNESSES ARE NOMINAL OR TYPICAL DIMENSIONS. THE ACTUAL DIMENSIONS TO BE CONSTRUCTED MAY BE VARIED TO FIT EXISTING CONDITIONS AS DIRECTED OR APPROVED BY THE ENGINEER. IT IS NOT INTENDED THAT EXISTING PAVEMENT OR SHOULDERS BE WIDENED, EXCEPT AS SPECIFIED FOR RAMPS D AND D1.

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PERFORATED PIPE DITCH

BACKFILL OF THE PERFORATED PIPE DITCH IS TO BE INCIDENTAL TO "PERFORATED PIPE - 4 INCH."

CLEANING OF CULVERT INLETS

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CHANGEABLE MESSAGE BOARDS

CHANGEABLE MESSAGE BOARDS FURNISHED BY THE CONTRACTOR SHALL BE RETAINED BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT.

NEW GUARDRAIL PLACEMENT

GUARDRAIL, END TREATMENTS AND TERMINAL SECTIONS TO BE REPLACED ARE LISTED BY STATIONS. EXACT PLACEMENT TO BE APPROVED BY THE ENGINEER ON CONSTRUCTION. GUARDRAIL POSTS ARE NOT TO IMPACT EXISTING LIGHTING CONDUITS OR PAVEMENT EDGE DRAINS.

SHORT-TERM LANE CLOSURES

SHORT-TERM LANE CLOSURES WILL BE ALLOWED ON US 41A. COSTS INCIDENTAL TO MAINTENANCE OF TRAFFIC.

LANE CLOSURES

THE NUMBER OF LANE CLOSURES SET UP FOR THE I-24 CONSTRUCTION ARE BASED UPON A CONCEPTUAL PROGRESSION OF THE PROJECT. THE CONTRACTOR WILL NOT BE PAID AN ADDITIONAL NUMBER OF LANE CLOSURES SHOULD ANY ALTERATION OF THE PROJECT PHASING BE DESIRABLE FOR THE CONTRACTOR'S PROGRESSION OF WORK UNLESS REVIEWED AND APPROVED BY THE ENGINEER.

EXISTING SIGNS

THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THE EXISTING SHEET SIGNS THROUGHOUT THE LIMITS OF THIS PROJECT. PAYMENT FOR THIS IS INCIDENTAL TO THE BID ITEM "MAINTAIN AND CONTROL TRAFFIC".

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PROJECT ACCESS

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PIPELINE VIDEO INSPECTION

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MOWING CONTRACT

THERE WILL BE A MOWING CONTRACT THROUGH THIS PROJECT. THE CONTRACTOR WILL ALLOW THE MOWING CONTRACTOR TO COMPLETE THE MOWING THROUGH THIS AREA.

COMPLETION DATE

THE SPECIFIED COMPLETION DATE FOR THIS PROJECT IS NOVEMBER 15, 2011. ALL ASPHALT SURFACING SHALL BE COMPLETED BY NOVEMBER 1, 2011, CONTRARY TO SECTION 108.09 OF THE STANDARD SPECIFICATIONS. LIQUIDATED DAMAGES WILL BE ASSESSED FROM DECEMBER THROUGH MARCH.

CONSTRUCTION SCHEDULE

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LABOR DAY WEEKEND SEPTEMBER 3RD - 5TH, 2011
THANKSGIVING HOLIDAY NOVEMBER 24TH - 27TH, 2011
CHRISTMAS HOLIDAY DECEMBER 24TH - 26TH, 2011
NEW YEAR'S HOLIDAY DECEMBER 31, 2011 - JANUARY 1ST, 2012

LIQUIDATED DAMAGES AND DISINCENTIVES

I-24 AND US 41A ARE TO REMAIN OPEN AT ALL TIMES THROUGHOUT CONSTRUCTION.

DAMAGES FOR CONSTRUCTION SHALL BE ASSESSED AT A RATE OF \$1,000 PER HOUR FOR EACH HOUR THE PHASE IS IN VIOLATION OF THE TIME FRAMES DESCRIBED. AFTER THE FIRST 24 HOURS THE CONTRACTOR WILL BE ASSESSED \$24,000 PER DAY.

JOINT SEALING

ALL JOINT SEALING SHALL BE PERFORMED USING HOT POURED ELASTIC JOINT SEALERS, IN ACCORDANCE WITH SECTION 807 OF THE SPECIFICATIONS, REGARDLESS OF THE WIDTH OF THE JOINT. ANY ADDITIONAL MATERIALS, LABOR, AND EQUIPMENT REQUIRED TO SEAL THE JOINTS DUE TO VARIATIONS IN JOINT WIDTH SHALL BE INCIDENTAL TO THE BID ITEMS: SAW, CLEAN & RESEAL TRANSVERSE JOINTS AND SAW, CLEAN & RESEAL LONGITUDINAL JOINTS.

DIAMOND GRINDING SLURRY

THE DIAMOND GRINDING SLURRY MAY BE WASTED ON THE SHOULDERS WHERE POSSIBLE.

MEDIAN CROSS-OVERS ON US 41A

ALL QUANTITIES RELATED TO MEDIAN CROSS-OVER CONSTRUCTION ARE INCORPORATED IN THE INDIVIDUAL BID ITEMS. THE QUANTITIES ARE DESIGNATED AS SUCH ON THE GENERAL SUMMARY BY NOTES. ON THE PAVING SUMMARY THE QUANTITIES ARE SEPARATE FROM THE US 41A QUANTITIES.

GENERAL NOTES

FILE NAME: G:\PWORK\VALEXI.SMITH\00144517\KYTC-SHEET.CEL

USER: Alexi.Smith
DATE PLOTTED: January 1, 2011

E-SHEET NAME:

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